



## CITY OF PATASKALA BOARD OF ZONING APPEALS

City Hall, Council Chambers  
621 West Broad Street  
Pataskala, Ohio 43062

### STAFF REPORT

April 13, 2020

#### Appeal Application AP-21-001

<b>Applicant:</b>	TS Tech USA, Corporation
<b>Owner:</b>	TS Tech USA, Corporation
<b>Location:</b>	6330 Taylor Road
<b>Acreage:</b>	21.01 acres
<b>Zoning:</b>	M-1 – Light Manufacturing
<b>Request:</b>	Requesting an appeal pursuant to Section 1211.03 of the Pataskala Code for a driveway permit application.

#### Description of the Request:

The Applicant is appealing the decision of the Planning and Zoning Department to disapprove a driveway permit application that would remove the conditions/restrictions for use as emergency access only.

#### Staff Summary:

On September 8, 2016 the Planning and Zoning Department approved a driveway permit application (16-356) for TS Tech to allow a driveway to access Taylor Road with conditions/restrictions that it be used for emergency access only. A copy of the permit and the conditions/restrictions is attached to the staff report.

On June 21, 2019, the Planning and Zoning Department approved a new commercial construction application (19-251) for the cross dock facility with the same conditions/restrictions that the driveway would be emergency access only. A copy of the permit and the conditions/restrictions is attached to the staff report.

The applicant is appealing the Planning and Zoning Department's decision to disapprove a driveway permit application due to traffic and safety concerns.

#### Staff Review:

City of Pataskala Codified Ordinance Section 1211.03 (Appeals) instructs the Clerk to, "in addition to transmitted the record to the [BPA] Board, provide a summary of the matter to the City Administrator in order for the City to determine whether comments contemplated by Section 1211.01 is considered appropriate" Section 1211.01 states "Council may, through the various administrative departments of the City, provide such information to the Board as may be appropriate to assist the Board in its considerations particularly as such considerations might concern technical matters of which the administration might have specific or detailed information that should be considered in the review of the Board. Any such information provided by the administrative departments shall be forwarded to

the Board in advance of a scheduled hearing and shall be presented in a neutral and objective manner, and not in the form of a recommendation to approve or reject.”

TS Tech’s appeal raises several technical matters that should be addressed by the administrative departments to assist the BZA in its considerations of this appeal. They are as follows:

1. TS Tech’s Notice of Appeal states by removing “all conditions imposed by Application Permit No 16-356 for unrestricted ingress and egress, [it does] not require trucks to perform any illegal or improper maneuvers pursuant to the Ohio Commercial Driver License Manual and relevant laws and regulations.” Notice of Appeal at ¶ 8. Presuming this is true – what effect, if any, does this have on the concerns raised by Alan Haines in his November 26, 2019 email to Erin Wiggins (at TS Tech) or February 25, 2021 email to Scott Fulton?
  - a. Heretofore, the legality of the movements had not been questioned, so this comment seemed irrelevant. However, the March 20, 2020 Access Study, which I have no record of receiving or reviewing until this most recent submittal in March of 2021, admits that trucks would need to cross the center-line of Taylor Road to exit the drive. The study attempts to validate this movement, but if this is legal, I cannot see a scenario where it is acceptable. It should be noted that one of the iterations proposed by TS Tech did have trucks going from the subject emergency access drive, out on to Taylor Road, then onto Cypress St. With the distance between those 2 drives being only about 80’, that proposed movement would not have been acceptable without additional controls.
2. TS Tech’s Notice of Appeal states it “requested that City personnel specifically identify relevant zoning and traffic concerns which are violated upon the proposed use, or identify improvements that would support the proposed usage.” Notice of Appeal at ¶ 8. Did the City have any additional or different comments, concerns, and/or potential solutions for TS Tech, other than those listed by Alan Haines in his November 26, 2019 email to Erin Wiggins? Did the City have any additional or different comments, concerns, and/or potential solutions for TS Tech, other than those listed in Alan Haines in his February 25, 2021 email to Scott Fulton?
  - a. Part of this question assumes that the onus of providing solutions is on the City. It is not. A number of issues were identified, and to help TS Tech understand how they could work with the City to address them, a number of alternative solutions were provided.
3. TS Tech’s January 27, 2021 Driveway Permit Application (Notice of Appeal, Exhibit A), states TS Tech “requests full access to and use of the private driveway and public roadway.” TS Tech’s Supplement to Driveway Permit Application states the “traffic study demonstrates that lifting the conditions of the permit will not degrade traffic operations on Taylor Road SW, and would

provide an improvement in traffic operations for traffic signals on Broad Street, especially Broad Street/Taylor Road SW intersection.” (Notice of Appeal, Exhibit A, . 8).

- a. I do not concur with the statements as made. The statement that the reduced traffic would improve the intersection operations at Broad and Taylor may not be technically incorrect, but is effectively insignificant. The numbers quoted are less than a 1% change, or in other words, less than a typical margin of error. As for not degrading traffic on Taylor Road, I disagree with this statement as well. This statement appears to be make an invalid jump from “acceptable level of service (LOS)” to no degradation. To clarify, the analysis shows a level of service C (acceptable LOS) after opening the drive, but without the drive, no designation is given. Going from no level of service gradation (which I would assume at a level of service A, since there is nothing to grade) to a level of service C is a degradation.

TS Tech submitted a Traffic Access Study dated March 20, 2020 (TS Tech Exhibit A, p. 27). The proposed conditions underling this Study indicate “the new outbound driveway to Taylor Road would only be open during the afternoon shift change period, approximately 3:00-4:00 PM. During the remainder of the day, the driveway would be gated and locked.” (TS Tech Exhibit A, p. 28) Does the March 20, 2020 Traffic Access Study support TS Tech’s Driveway Permit Application and its request for “full access to and use of the private driveway and public roadway”? If not, does this matter and why? Does the March 20, 2020 Traffic Access Study address the comments, concerns, and/or potential solutions listed in Alan Haines’s November 26, 2019 email to Erin Wiggins or February 25, 2021 email to Scott Fulton? If not, why?

- a. The first item of note is that I have no records of having previously received the Access Study as included in the March 16<sup>th</sup>, 2021 “Exhibit A” document.

Withstanding that, I have reviewed the report and do take exception to a few items:

1. The provided studies assume a peak hour, and average traffic across the peak hour, which is a standard method of analysis for a typical traffic study; however, the distribution of traffic during a shift change, as proposed by TS Tech, does not happen as an average across a peak hour. Instead, and as observed during a site visit, the shift change traffic appears to occur in a period of about 10 to 15 minutes. As the proposed, averaged numbers in the study do not account for this, the conclusions drawn cannot be validated.
2. Gap analysis is mentioned in the report, but I do not see any back-up to this statement. However, even if gap analysis was performed, if the methodology averaged the traffic across the hour, versus the actual shift change timeframe as mentioned previously, the results would not be accurate.

3. The study does actually validate one of the concerns, which is that if trucks are allowed to exit from the existing emergency access drive, the AutoTurn analysis shows that they will cross left of center to make this movement. The study claims that this is not an issue due to sufficient gaps in traffic, but I do not support this conclusion.
  - a. Knowingly allowing a truck turning movement to cross the center line is not safe, is poor precedent, and I cannot see how this would be acceptable under any conditions.
  - b. Adequate gaps in traffic, even if appropriately accounted for, do not validate this movement as acceptable.
4. The proposed conditions of the study state that the drive would only be open from 3:00 to 4:30 p.m. for shift change traffic, but then goes on to address trucks. My understanding is that TS Tech is now requesting full access. None of these things are consistent with each other, which only serves to further my concerns with this proposal.
  - a. The studies do not match the proposal. If the studies don't match the proposal, how can any of the information be validated or trusted?
  - b. The study addresses outbound traffic only and is not consistent with the request.
  - c. The provided study is based on usage from 3:00 pm to 4:30 pm. If this is still the proposal, who will police/enforce the time constraints? If it is not the proposal, the study is not applicable to the request.
5. In summary, the arguments and information provided are inconsistent, do not address the traffic concerns, and do not support the request to remove the emergency access designation.
6. For these reasons, I strongly oppose any change from the emergency access designation that is currently in place.

Additionally, both the Pataskala Police Chief and the Licking Heights School District have expressed concerns with the request. Full comments are attached.

**Resolution:**

For your convenience, the following resolution may be considered by the Board of Zoning Appeals when making a motion:

"I move to approve the applicant's appeal pursuant to Section 1211.03 of the Pataskala Code because the Planning and Zoning Department should have approved the driveway permit application removing the restrictions for emergency access only.



## CITY OF PATASKALA PLANNING & ZONING DEPARTMENT

621 West Broad Street, Suite 2A  
Pataskala, Ohio 43062

### DRIVEWAY PERMIT APPLICATION

(Pataskala Codified Ordinances Sections 1121.13 and 1291.02)

Property Information		Staff Use	
Address: 0 Taylor Rd Pataskala, Ohio		Application Number:	
Parcel Number: 201503300005792		16-356	
Zoning: M-1	Acres: 21.01 Acres	Fee:	
063-140916-00.000		\$30.00	
Applicant Information		Filing Date:	
Name: Stimmel Construction Co.		8-22-16	
Address: 8905 Vanatta Rd		Receipt Number:	
City: Logan	State: OH	20202	
Zip: 43138	Phone: 740.380.9800	Email: info@stimmelconstruction.com	
Owner Information		Documents	
Name: T S Tech U.S.A. Corp		<input checked="" type="checkbox"/> Application	
Address: 8400 E. Broad St		<input checked="" type="checkbox"/> Fee	
City: Reynoldsburg	State: OH	<input checked="" type="checkbox"/> Site Plan	
Zip: 43068	Phone: 614-575-4824		
Email:			
Driveway Information			
Describe the Project (driveway dimensions, location, etc.)			
Widen Existing Drive			
Documents to Submit			
Driveway Application: Submit 1 copy of the driveway permit application.			
Fee: Application fee of \$30.00			
Site Plan: Submit two (2) copies of a site plan showing the following:			
<ul style="list-style-type: none"><li>Location of all existing buildings and structures</li><li>Driveway location and dimensions</li><li>Driveway distance to property lines</li><li>Property lines, easements and right-of-way</li></ul>			

### Important Information

**Applicability:** A driveway permit is required for all driveways and driveway aprons.

**Driveway Regulations:** All driveways must meet the following regulations:

- Driveways must have a maximum grade of 10 percent.
- Driveways and curb cuts must be located at least three (3) feet from the side lot line.
- Driveways must have a minimum width of 10 feet.
- The curb cut flare or apron must be three (3) feet wider than the driveway on each side.
- Driveways in a platted subdivision must be hard-surfaced with asphalt, cement or pavers.
- Driveways must be graded and maintained so water does not accumulate or drain onto an adjacent property.
- It is the property owner's responsibility to determine property lines and certify the driveway meets the appropriate setbacks at the time of installation or construction.

#### Home Owners Association:

- It is important to check with the neighborhood Home Owners Association, if applicable, for any driveway requirements they may have.
- Driveways may require approval from the Home Owners Association prior to construction.

#### Inspections:

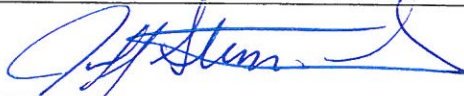
- The Pataskala Planning and Zoning Department will inspect the driveway twice:
  1. First inspection once the driveway forms are placed.
  2. Second inspection upon completion to ensure that the driveway complies with the approved permit.
- To schedule an inspection please call the Zoning Inspector at 740-927-3885.

**Public Service Department:** The Public Service Department will review the driveway permit application if work is to be done in the right of way.

### Signatures

I certify the facts, statements and information provided on and attached to this application are true and correct to the best of my knowledge and understand the requirements for a driveway. Also, I authorize City of Pataskala staff to inspect the property as necessary as it pertains to this application.

Applicant:



Date:

8-22-2016

Owner:



Date:

8-22-2016

### Zoning Inspector

☒ Approved ☐ Disapproved

Zoning Inspector:



Approval Date:

9-8-16

Expiration Date:

9-8-17

Conditions:

See attached



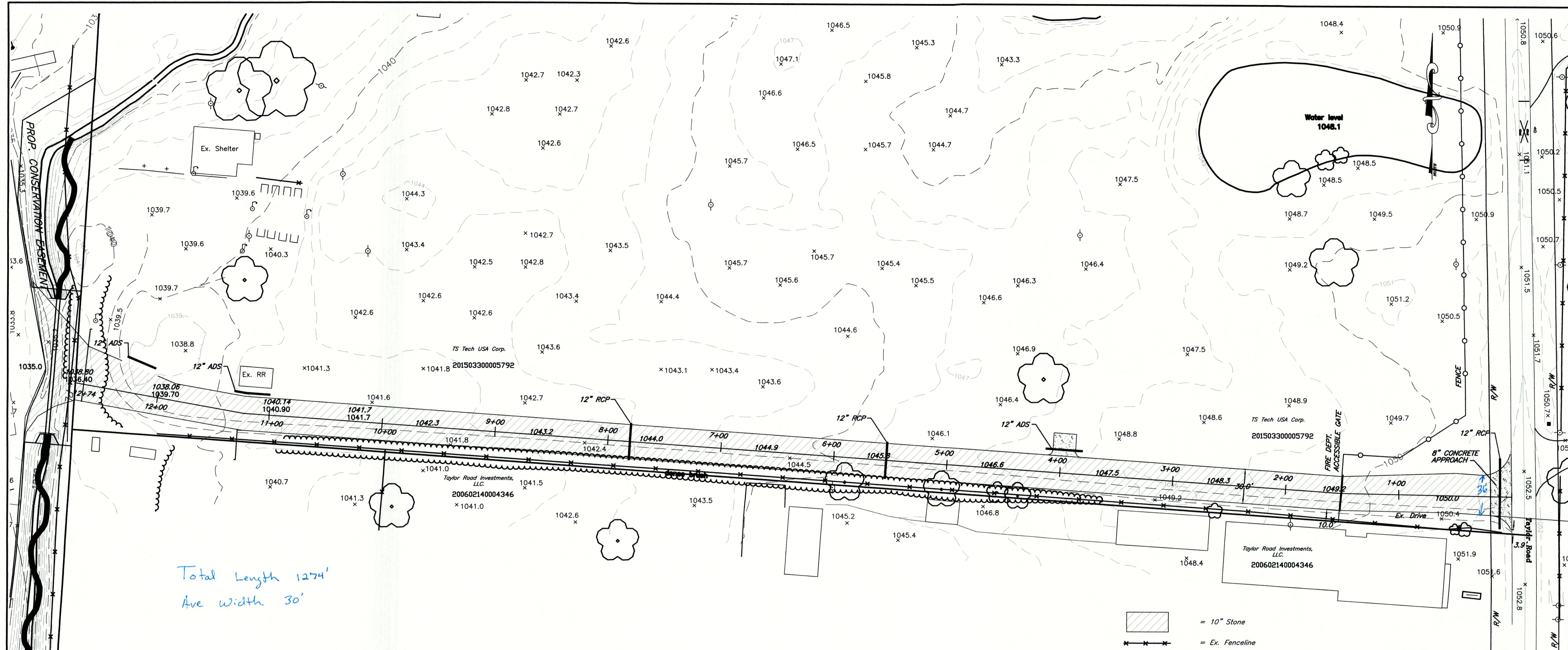
## **CITY OF PATASKALA PLANNING & ZONING DEPARTMENT**

621 West Broad Street, Suite 2A  
Pataskala, Ohio 43062

### **CONDITIONS OF APPROVAL**

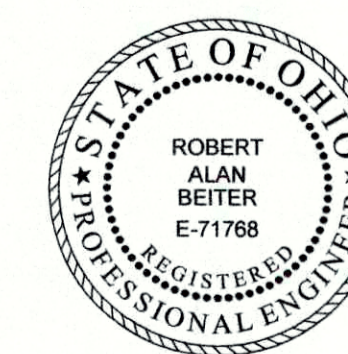
Driveway Permit Application 16-356

1. The driveway shall be used for emergency access only. Access by garbage trucks, semi-trucks, employee automobiles or other nonemergency vehicles is prohibited.
2. The gate shall remain locked at all times and shall only be accessible by emergency personnel and applicable TS Tech employees.
3. A right-of-way permit shall be required for any improvements in the right-of-way along Taylor Road.
4. The driveway gate and the fence along Taylor Road shall not exceed four (4) feet in height and a fence permit must be obtained from the Pataskala Planning and Zoning Department prior to installation.
5. All necessary approvals shall be obtained from the City of Pataskala if the driveway is to be altered or upgraded to full access.
6. Failure to abide by these conditions shall constitute a violation of Chapter 1209 of the Pataskala Code.



Total Length 1274'  
Ave width 30'

**APPROVED**  
Alt Hc 9-8-16  
#16-356



<b>CROSSING WATERS ENGINEERING, INC.</b> P.O. BOX 27, 280 S. MAIN ST., SUITE A SUGAR GROVE, OHIO 43155 (740) 746-0250	
<b>TSTech MANUFACTURING FACILITY</b> 8400 EAST BROAD STREET JEFFERSON TWP., FRANKLIN COUNTY, OHIO <b>EMERGENCY ACCESS DRIVE PLAN</b>	
CONTRACT	JOB NO.: CW-15010
DRAWN BY: RAB	SCALE: 1" = 40'
CHECKED BY: JAS	DATE: 08/01/2016
APPROVED BY: RAB	SHEET: 1 OF 1

**CROSSING WATERS ENGINEERING,**

P.O. Box 27  
280 S. Main St., Suite A  
Sugar Grove, Ohio 43155  
(740) 746-0250  
www.crossingwaterseng.com

NO.	REVISION	BY	DATE	RECORD	DRAWING

# City of Pataskala

*Alan W. Haines, P.E.*  
*Director of Public Services*

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621 West Broad Street  
Pataskala, OH 43062  
Office (740) 927-0145  
Fax (740) 927-0228

September 8, 2016

Stimmel Construction Co.  
8905 Vanatta Rd  
Logan, Ohio 43138

Dear Applicant:

Attached is the approved permit to allow Stimmel Construction Company to widen an existing driveway and concert an apron at 0 Taylor Rd. Read it carefully and comply with the provisions stated therein, including the date of completion.

All costs for the construction and maintenance of this installation shall be borne by the applicant or his successor in title.

The City of Pataskala and the Department of Transportation be saved harmless from any claims arising as a result of granting said permit.

This permit is granted and enforced under Ordinance 98-3213 of the City of Pataskala, and will be revoked anytime work is found to be non-compliant with the conditions imposed therein.

Please contact the Public Service Department before work is started at 740-927-0145.

Sincerely,



Alan W. Haines, P.E.  
Director of Public Services

PATASKALA  
DEPARTMENT OF TRANSPORTATION  
PERMIT

Subject to all of the terms, conditions and restrictions printed or written below, and on the following pages hereof, permission is hereby granted to: Allow Stimmel Construction Company to widen an existing driveway and concert an apron at 0 Taylor Rd. Traffic must be maintained within the work zone. **Restoration shall be according to City of Columbus, Department of Public Service Division of Engineering and Construction Pavement & Utility Cut Repair Standards. Restoration is required to be completed with in 5 working days. All pavement disrupted as a result of the work listed above must be restored to the satisfaction of the City of Pataskala. Asphalt and pavement joint must be sealed.**

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This permit is to be in possession of employees in charge of work at all times. To be shown upon request to any employee of the Department of Transportation.

The permittee shall be responsible for any damage to the street right-of-way resulting from this installation. This work will be preformed at no cost to the City of Pataskala. During this installation complete safety precautions are to be exercised so as to avoid any inconvenience or danger to the traveling public. There shall be no parking of vehicles or stockpiling of equipment or materials on the right-of-way. It is the responsibility of the Permittee to contact the **OHIO UTILITIES PROTECTION SERVICE (OUPS)** prior to digging in the street right-of-way.

It is understood that the Permittee or his successors in title will be responsible for the maintenance of this installation.

Upon completion of this work, the Permittee shall restore the street right-of-way to a condition, which is equal to or better than that which originally existed. (Including seeding and mulching)

It is essential that the Director of Public Services be notified at least 24 hours prior to the start of this project. This permit is not a substitute for satisfying the rights of any other party that may have an interest in the easement (or underlying fee).

This permit shall be void unless the work herein contemplated shall have been completed before:

OCTOBER 8<sup>th</sup>, 2016

DATED THIS 8<sup>th</sup> DAY OF SEPTEMBER 2016.



Alan W. Haines, P.E.  
Director of Public Services

**\*NOTE: TWO WORKING DAYS BEFORE YOU DIG  
CALL TOLL FREE 1-800-362-2764  
OHIO UTILITIES PROTECTION SERVICE**

GENERAL PROVISIONS IN FORCE IN SO FAR AS THEY APPLY TO THE WORK  
TO BE DONE UNDER THIS PERMIT  
GENERAL PROVISIONS APPLICABLE TO ALL PERMITS

The granting of this permit does not in any way abridge the right of the Director in his jurisdiction over Municipal Streets. If, in the process of any future work or for the benefit of the traveling public, it becomes necessary, in the opinion of the Director to order the removal, reconstruction, relocation, or repair of any of the fixtures, or work performed under this permit, said removal, reconstruction relocation, or repair shall be wholly at the expense of the owners thereof, and be made as directed by the Director.

The Director shall act for and in behalf of the City of Pataskala in the issuance of and the carrying out of the provisions of all permits.

The Director shall have full authority to insure that the provisions of this permit are fully complied with; and retains the right to reject any materials or workmanship in the restoration of Municipal facilities. Failure on the part of the permittee to conform to the provisions of this permit will be cause for suspension, revocation or annulment of this permit, as the Director deems necessary.

If the party or parties to whom this permit is issued does anything contrary to the orders of the Director and after due notice, fails to correct such work or to remove such structure or material as he or they may be ordered to remove, the Department of Transportation may with or without notice, correct such work or remove such structures or material; and the party or parties to whom this permit is issued shall reimburse the Department of Transportation for any expense incurred in correcting the work or removing the structure or materials.

The right is reserved, during the time any or all the work is being performed, to appoint an inspector over the work who shall represent the interest of the City on the work, and Any compensation arranged for shall be paid wholly by the permit holder. All the work herein contemplated shall be done under the supervision and to the satisfaction of the Department of Transportation, and the entire expense thereof, shall be borne by the party or parties to whom this permit is issued.

Except as herein authorized, no excavation shall be made or obstacle placed within the limits of the street in such manner as to interfere with the travel over the road.

If any grading, sidewalk, or other work done under this permit interferes with drainage of the roads in any way, such catch basins and outlets shall be constructed as may be necessary to take proper care of said drainage.

The acceptance of this permit or the doing of any work thereunder shall constitute an agreement by the party or parties to whom the permit is granted to comply with all of the conditions and restrictions printed or written herein.

All the above conditions shall be applicable to the work herein authorized, unless the same are inconsistent with the conditions on the face of the permit, in which case the conditions written or printed on the face of the permit shall apply.

City property, disturbed by the permittee, shall be restored using materials, design and workmanship in conformance with the Ohio Department of Transportation Construction and Material Specifications, Location and Design Manual or other existing Department Standards.

All pole lines are to be built in accordance with Rule 4901:3-1-08 of the Ohio Administrative Code promulgated and enforced by the Public Utilities Commission of Ohio.

All work requiring men or vehicles on the pavement or shoulders shall comply with all of the requirements of the Ohio Manual of Uniform Traffic Control Devices and Item 614 (Maintaining Traffic) of the Ohio Department of Transportation Construction and Material Specifications. Failure to comply with this requirement will be cause for immediate suspension of the permit until the proper traffic controls have been provided.

The permittee upon completion of the work shall leave the highway clean of all rubbish, excess materials, temporary structures, and equipment, and all parts of the road shall be left in an acceptable condition.

The permittee shall save harmless the City of Pataskala and all of its representatives from all suits, actions, or claims of any character, brought on account of any injuries or damages sustained by any person or property in consequence of any neglect or on account of any act or omission as a result of the issuance of this permit.

The permittee shall comply with the Air Pollution requirements of Rule 3745-17-08 of the Ohio Administrative Code promulgated and enforced by the Ohio Environmental Protection Agency.

PD 8/22/16

City of Pataskala  
Public Service Department  
www.ci.pataskala.oh.us

# CITY OF PATASKALA, OHIO



621 West Broad Street  
Suite 2B  
Pataskala, Ohio 43062  
Telephone: 740-927-2021  
Fax: 740-927-0228

Permit # 2016000

**Fee: \$15.00**

## Right-of-Way Work Permit Application (Ordinance 905.02)

Applicant: Stimmel Construction Co. Date: 8-22-16

Applicant's address: 8905 Vanatta Rd Logan, Ohio 43138  
(Street, City, Zip code)

Applicant's telephone #: 740-380-9800 Applicant's fax #: 740-380-9801

Property owner: TS Tech U.S.A. Corp.

Property owner's address: 8400 E. Broad St. Reynoldsburg, Oh 43068  
(Street, City, Zip code)

Property owner's telephone #: 614-575-4824

Location of project: 0 Taylor Rd. Pataskala, Ohio 43063

**Please attach a copy of the project plans and submit with this application (if applicable).**

Describe the work to be completed, **including materials to be used:**  
(i.e. pipe type, fill material, etc...)

Widen Existing Drive  
1. RCP Pipe  
2. Concrete Entrance

The above applicant agrees to adhere to all sections of City of Pataskala Codified Ordinances, Chapter 905, as it relates to work in the right-of-way.

If the proposed construction/installation requires the opening of the pavement, you must provide the following information:

info@stimmelconstruction.com

1. Conditions necessitating opening of pavement: None

2. The opening to be made in the pavement will be \_\_\_\_\_ feet long by \_\_\_\_\_ feet wide and \_\_\_\_\_ feet deep.

3. Pavement will be replaced by (list contractor) \_\_\_\_\_ to the satisfaction of the Director of Public Services.

The applicant hereby agrees to backfill the trench of said opening in compliance with the standards of the Ohio Department of Transportation using construction methods, materials, and workmanship as prescribed in the Construction Materials Specifications (Current Edition) of the Department of Transportation.

*All work requiring workers or vehicles on the pavement or roadway shoulders shall comply with all of the Ohio Manual of Uniform Traffic Control Devices and item 614 (Maintaining Traffic) of the Ohio Department of Transportation construction and material specifications. Failure to comply with this requirement will be cause for immediate suspension of this permit until proper traffic controls have been implemented.*

**48 HOURS PRIOR TO EXCAVATION, ALL PERMITTEES MUST NOTIFY THE OHIO UTILITIES PROTECTION SERVICE AT 1-800-362-2764.**

We agree to comply with all of the conditions, restrictions, and regulations of the Department of Transportation and the City of Pataskala.

Stimmel Construction Co.  
Applicant's name (print) Jeff Stimmel

8/22/2016  
Date

[Signature]  
Applicant's signature



## CITY OF PATASKALA PLANNING & ZONING DEPARTMENT

621 West Broad Street, Suite 2A  
Pataskala, Ohio 43062

### NEW COMMERCIAL CONSTRUCTION APPLICATION

(Pataskala Codified Ordinances Section 1209)

Property Information		
Address: 6630 Taylor Road SW		
Parcel Number: 063-140916-00.000		
Development Name: TS Tech Cross Dock		
Lot/Building Number: 6630	Zoning: M-1, Manufacturing	Acres: 21.01
Water Supply:		
<input type="checkbox"/> City of Pataskala	<input checked="" type="checkbox"/> South West Licking	<input type="checkbox"/> On Site
Wastewater Treatment:		
<input type="checkbox"/> City of Pataskala	<input checked="" type="checkbox"/> South West Licking	<input type="checkbox"/> On Site

Staff Use
Application Number: 19-251
Fee: \$1050
Filing Date: 5-31-19
Receipt Number: 499803
<input checked="" type="checkbox"/> Auditor Notified

Applicant Information		
Name: Rick C. White		
Address: 174 E Long Street, Suite 300		
City: Columbus	State: Ohio	Zip: 43215
Phone: (937)592-0420	Email: rwhite@redarchitects.com	

Owner Information		
Name: TS Tech Corporation		
Address: 8400 East Broad Street		
City: Reynoldsburg	State: Ohio	Zip: 43068
Phone: 1-614-577-1088	Email: erin.wiggins@tstech.com	

Documents
<input checked="" type="checkbox"/> Application
<input checked="" type="checkbox"/> Fee
<input checked="" type="checkbox"/> Site Plan
<input checked="" type="checkbox"/> Elevations
<input checked="" type="checkbox"/> Erosion & Sediment Control Plan
<input checked="" type="checkbox"/> Compliance Application
<input type="checkbox"/> Board of Health Approval - If applicable

Documents to Submit
<b>New Commercial Construction Application:</b> Submit one (1) copy of the New Commercial Construction application.
<b>Fee:</b> Application fee of \$1,050.00
<b>Site Plan:</b> Submit one (1) copy of a site plan that includes all required information.
<b>Elevations:</b> Submit one (1) copy of building elevations that includes all required information.

**Erosion and Sediment Control Plan:** Submit one (1) copy of Erosion & Sediment Control plan that includes all required information.

**Certificate of Compliance Application:** Submit one (1) copy of the Commercial Compliance application.

**Board of Health Approval:** Approval from the Licking County Board of Health indicating that the property is suitable for a well and septic system if the property will not be served by central water and sewer.

### Important Information

**Applicability:** A New Commercial Construction Application is required for all new commercial, industrial and institutional construction.

**Building Permit:**

- All new commercial construction will require a building permit from the Licking County Building Department.
- Please call the Licking County Building Department at 740-349-6671 for additional information.

**Inspections:**

- The Pataskala Planning and Zoning Department will inspect the property after construction has been completed.
- To schedule an inspection please call the Zoning Inspector at 740-927-3885.

### Site Superintendent

Name: Paul Crissinger

Title: Project Manager

Phone: (937)539-1326

Email: pcrissinger@humbleconstruction.com

### Signatures

I certify the facts, statements and information provided on and attached to this application are true and correct to the best of my knowledge and understand the requirements for new commercial construction. Also, I authorize City of Pataskala staff to inspect the property as necessary as it pertains to this application.

Applicant:



Date:

May 23, 2019

Owner:



Date:

5/23/2019

### Zoning Inspector

☒ Approved ☐ Disapproved

Zoning Inspector:



Approval Date:

6-21-19

Expiration Date:

6-21-20

Conditions:

See attached.



## **CITY OF PATASKALA PLANNING & ZONING DEPARTMENT**

621 West Broad Street, Suite 2A  
Pataskala, Ohio 43062

### **CONDITIONS OF APPROVAL**

New Commercial Construction Application 19-251

1. Access shall remain emergency access only, and subject to the conditions of Zoning Permit #16-356 (Attached).
2. The Applicant shall submit a mylar copy of the submitted plans with the following signature lines on the title sheet:
  - Applicant:
  - Director, S.W.L.C.W.S.:
  - Director of Planning, City of Pataskala:
  - Public Service Director, City of Pataskala:
  - City Administrator, City of Pataskala:



# CITY OF PATASKALA PLANNING & ZONING DEPARTMENT

621 West Broad Street, Suite 2A  
Pataskala, Ohio 43062

## DRIVEWAY PERMIT APPLICATION

(Pataskala Codified Ordinances Sections 1121.13 and 1291.02)

Property Information		Staff Use	
Address: 0 Taylor Rd Pataskala, Ohio		Application Number:	
Parcel Number: 201503300005792		16-356	
Zoning: M-1	Acres: 21.01 Acres	Fee:	
063-140916-00.000		\$30.00	
Applicant Information		Filing Date:	
Name: Stimmel Construction Co.		8-22-16	
Address: 8905 Vanatta Rd		Receipt Number:	
City: Logan	State: OH	20202	
Zip: 43138	Phone: 740-380-9800	Email: info@stimmelconstruction.com	
Owner Information		Documents	
Name: TS Tech U.S.A. Corp		<input checked="" type="checkbox"/> Application	
Address: 8400 E. Broad St		<input checked="" type="checkbox"/> Fee	
City: Reynoldsburg	State: OH	<input checked="" type="checkbox"/> Site Plan	
Zip: 43068	Phone: 614-575-4824		
Email:			
Driveway Information			
Describe the Project (driveway dimensions, location, etc.)			
Widen Existing Drive			
Documents to Submit			
Driveway Application: Submit 1 copy of the driveway permit application.			
Fee: Application fee of \$30.00			
Site Plan: Submit two (2) copies of a site plan showing the following:			
<ul style="list-style-type: none"><li>Location of all existing buildings and structures</li><li>Driveway location and dimensions</li><li>Driveway distance to property lines</li><li>Property lines, easements and right-of-way</li></ul>			

### Important Information

**Applicability:** A driveway permit is required for all driveways and driveway aprons.

**Driveway Regulations:** All driveways must meet the following regulations:

- Driveways must have a maximum grade of 10 percent.
- Driveways and curb cuts must be located at least three (3) feet from the side lot line.
- Driveways must have a minimum width of 10 feet.
- The curb cut flare or apron must be three (3) feet wider than the driveway on each side.
- Driveways in a platted subdivision must be hard-surfaced with asphalt, cement or pavers.
- Driveways must be graded and maintained so water does not accumulate or drain onto an adjacent property.
- It is the property owner's responsibility to determine property lines and certify the driveway meets the appropriate setbacks at the time of installation or construction.

#### Home Owners Association:

- It is important to check with the neighborhood Home Owners Association, if applicable, for any driveway requirements they may have.
- Driveways may require approval from the Home Owners Association prior to construction.

#### Inspections:

- The Pataskala Planning and Zoning Department will inspect the driveway twice:
  1. First inspection once the driveway forms are placed.
  2. Second inspection upon completion to ensure that the driveway complies with the approved permit.
- To schedule an inspection please call the Zoning Inspector at 740-927-3885.

**Public Service Department:** The Public Service Department will review the driveway permit application if work is to be done in the right of way.

### Signatures

I certify the facts, statements and information provided on and attached to this application are true and correct to the best of my knowledge and understand the requirements for a driveway. Also, I authorize City of Pataskala staff to inspect the property as necessary as it pertains to this application.

Applicant:

Date:

8-22-2016

Owner:

Date:

8-22-2016

### Zoning Inspector

☒ Approved    ☐ Disapproved

Zoning Inspector:

Approval Date:

9-8-16

Expiration Date:

9-8-17

Conditions:

See attached



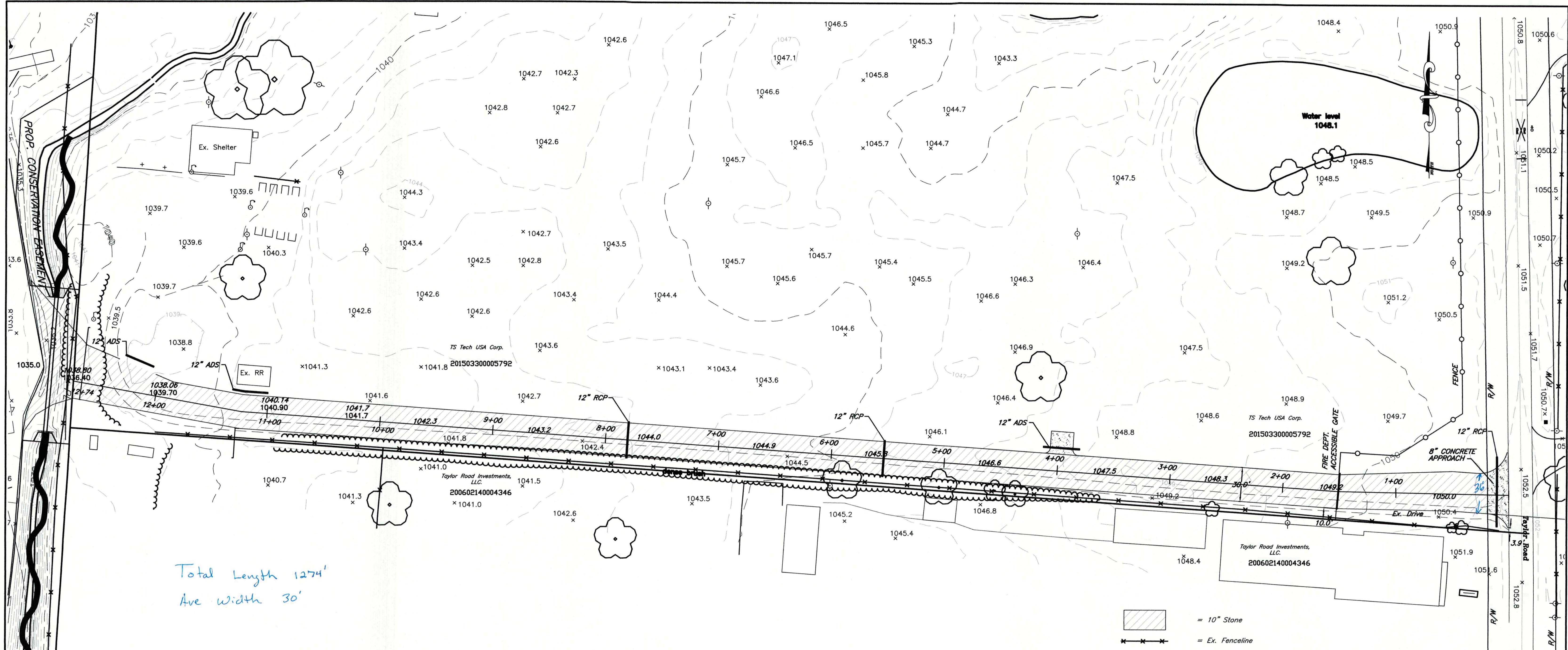
## **CITY OF PATASKALA PLANNING & ZONING DEPARTMENT**

621 West Broad Street, Suite 2A  
Pataskala, Ohio 43062

### **CONDITIONS OF APPROVAL**

Driveway Permit Application 16-356

1. The driveway shall be used for emergency access only. Access by garbage trucks, semi-trucks, employee automobiles or other nonemergency vehicles is prohibited.
2. The gate shall remain locked at all times and shall only be accessible by emergency personnel and applicable TS Tech employees.
3. A right-of-way permit shall be required for any improvements in the right-of-way along Taylor Road.
4. The driveway gate and the fence along Taylor Road shall not exceed four (4) feet in height and a fence permit must be obtained from the Pataskala Planning and Zoning Department prior to installation.
5. All necessary approvals shall be obtained from the City of Pataskala if the driveway is to be altered or upgraded to full access.
6. Failure to abide by these conditions shall constitute a violation of Chapter 1209 of the Pataskala Code.



Total Length 1274'  
Ave width 30'

**APPROVED**  
Alt Hc 9-8-16  
#16-356



**CROSSING WATERS ENGINEERING, INC.**

P.O. Box 27  
280 S. Main St., Suite A  
Sugar Grove, Ohio 43155  
(740) 746-0250  
www.crossingwaterseng.com

NO.	REVISION	BY	DATE	RECORD DRAWING

<b>CROSSING WATERS ENGINEERING, INC.</b> P.O. BOX 27, 280 S. MAIN ST., SUITE A SUGAR GROVE, OHIO 43155 (740) 746-0250	
<b>TSTech MANUFACTURING FACILITY</b> 8400 EAST BROAD STREET JEFFERSON TWP., FRANKLIN COUNTY, OHIO <b>EMERGENCY ACCESS DRIVE PLAN</b>	
CONTRACT	JOB NO.: CW-15010
DRAWN BY: RAB	SCALE: 1" = 40'
CHECKED BY: JAS	DATE: 08/01/2016
APPROVED BY: RAB	SHEET: 1 OF 1

# City of Pataskala

*Alan W. Haines, P.E.*  
*Director of Public Services*

---

621 West Broad Street  
Pataskala, OH 43062  
Office (740) 927-0145  
Fax (740) 927-0228

September 8, 2016

Stimmel Construction Co.  
8905 Vanatta Rd  
Logan, Ohio 43138

Dear Applicant:

Attached is the approved permit to allow Stimmel Construction Company to widen an existing driveway and concert an apron at 0 Taylor Rd. Read it carefully and comply with the provisions stated therein, including the date of completion.

All costs for the construction and maintenance of this installation shall be borne by the applicant or his successor in title.

The City of Pataskala and the Department of Transportation be saved harmless from any claims arising as a result of granting said permit.

This permit is granted and enforced under Ordinance 98-3213 of the City of Pataskala, and will be revoked anytime work is found to be non-compliant with the conditions imposed therein.

Please contact the Public Service Department before work is started at 740-927-0145.

Sincerely,



Alan W. Haines, P.E.  
Director of Public Services

PATASKALA  
DEPARTMENT OF TRANSPORTATION  
PERMIT

Subject to all of the terms, conditions and restrictions printed or written below, and on the following pages hereof, permission is hereby granted to: Allow Stimmel Construction Company to widen an existing driveway and concert an apron at 0 Taylor Rd. Traffic must be maintained within the work zone. **Restoration shall be according to City of Columbus, Department of Public Service Division of Engineering and Construction Pavement & Utility Cut Repair Standards. Restoration is required to be completed with in 5 working days. All pavement disrupted as a result of the work listed above must be restored to the satisfaction of the City of Pataskala. Asphalt and pavement joint must be sealed.**

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This permit is to be in possession of employees in charge of work at all times. To be shown upon request to any employee of the Department of Transportation.

The permittee shall be responsible for any damage to the street right-of-way resulting from this installation. This work will be performed at no cost to the City of Pataskala. During this installation complete safety precautions are to be exercised so as to avoid any inconvenience or danger to the traveling public. There shall be no parking of vehicles or stockpiling of equipment or materials on the right-of-way. It is the responsibility of the Permittee to contact the **OHIO UTILITIES PROTECTION SERVICE (OUPS)** prior to digging in the street right-of-way.

It is understood that the Permittee or his successors in title will be responsible for the maintenance of this installation.

Upon completion of this work, the Permittee shall restore the street right-of-way to a condition, which is equal to or better than that which originally existed. (Including seeding and mulching)

It is essential that the Director of Public Services be notified at least 24 hours prior to the start of this project. This permit is not a substitute for satisfying the rights of any other party that may have an interest in the easement (or underlying fee).

This permit shall be void unless the work herein contemplated shall have been completed before:

OCTOBER 8<sup>th</sup>, 2016

DATED THIS 8<sup>th</sup> DAY OF SEPTEMBER 2016.



Alan W. Haines, P.E.  
Director of Public Services

**\*NOTE: TWO WORKING DAYS BEFORE YOU DIG  
CALL TOLL FREE 1-800-362-2764  
OHIO UTILITIES PROTECTION SERVICE**

GENERAL PROVISIONS IN FORCE IN SO FAR AS THEY APPLY TO THE WORK  
TO BE DONE UNDER THIS PERMIT  
GENERAL PROVISIONS APPLICABLE TO ALL PERMITS

The granting of this permit does not in any way abridge the right of the Director in his jurisdiction over Municipal Streets. If, in the process of any future work or for the benefit of the traveling public, it becomes necessary, in the opinion of the Director to order the removal, reconstruction, relocation, or repair of any of the fixtures, or work performed under this permit, said removal, reconstruction relocation, or repair shall be wholly at the expense of the owners thereof, and be made as directed by the Director.

The Director shall act for and in behalf of the City of Pataskala in the issuance of and the carrying out of the provisions of all permits.

The Director shall have full authority to insure that the provisions of this permit are fully complied with; and retains the right to reject any materials or workmanship in the restoration of Municipal facilities. Failure on the part of the permittee to conform to the provisions of this permit will be cause for suspension, revocation or annulment of this permit, as the Director deems necessary.

If the party or parties to whom this permit is issued does anything contrary to the orders of the Director and after due notice, fails to correct such work or to remove such structure or material as he or they may be ordered to remove, the Department of Transportation may with or without notice, correct such work or remove such structures or material; and the party or parties to whom this permit is issued shall reimburse the Department of Transportation for any expense incurred in correcting the work or removing the structure or materials.

The right is reserved, during the time any or all the work is being performed, to appoint an inspector over the work who shall represent the interest of the City on the work, and Any compensation arranged for shall be paid wholly by the permit holder. All the work herein contemplated shall be done under the supervision and to the satisfaction of the Department of Transportation, and the entire expense thereof, shall be borne by the party or parties to whom this permit is issued.

Except as herein authorized, no excavation shall be made or obstacle placed within the limits of the street in such manner as to interfere with the travel over the road.

If any grading, sidewalk, or other work done under this permit interferes with drainage of the roads in any way, such catch basins and outlets shall be constructed as may be necessary to take proper care of said drainage.

The acceptance of this permit or the doing of any work thereunder shall constitute an agreement by the party or parties to whom the permit is granted to comply with all of the conditions and restrictions printed or written herein.

All the above conditions shall be applicable to the work herein authorized, unless the same are inconsistent with the conditions on the face of the permit, in which case the conditions written or printed on the face of the permit shall apply.

City property, disturbed by the permittee, shall be restored using materials, design and workmanship in conformance with the Ohio Department of Transportation Construction and Material Specifications, Location and Design Manual or other existing Department Standards.

All pole lines are to be built in accordance with Rule 4901:3-1-08 of the Ohio Administrative Code promulgated and enforced by the Public Utilities Commission of Ohio.

All work requiring men or vehicles on the pavement or shoulders shall comply with all of the requirements of the Ohio Manual of Uniform Traffic Control Devices and Item 614 (Maintaining Traffic) of the Ohio Department of Transportation Construction and Material Specifications. Failure to comply with this requirement will be cause for immediate suspension of the permit until the proper traffic controls have been provided.

The permittee upon completion of the work shall leave the highway clean of all rubbish, excess materials, temporary structures, and equipment, and all parts of the road shall be left in an acceptable condition.

The permittee shall save harmless the City of Pataskala and all of its representatives from all suits, actions, or claims of any character, brought on account of any injuries or damages sustained by any person or property in consequence of any neglect or on account of any act or omission as a result of the issuance of this permit.

The permittee shall comply with the Air Pollution requirements of Rule 3745-17-08 of the Ohio Administrative Code promulgated and enforced by the Ohio Environmental Protection Agency.

PD 8/22/16

City of Pataskala  
Public Service Department  
www.ci.pataskala.oh.us

# CITY OF PATASKALA, OHIO



621 West Broad Street  
Suite 2B  
Pataskala, Ohio 43062  
Telephone: 740-927-2021  
Fax: 740-927-0228

Permit # 2016000

**Fee: \$15.00**

## Right-of-Way Work Permit Application (Ordinance 905.02)

Applicant: Stimmel Construction Co. Date: 8-22-16

Applicant's address: 8905 Vanatta Rd Logan, Ohio 43138  
(Street, City, Zip code)

Applicant's telephone #: 740-380-9800 Applicant's fax #: 740-380-9801

Property owner: TS Tech U.S.A. Corp.

Property owner's address: 8400 E. Broad St. Reynoldsburg, Oh 43068  
(Street, City, Zip code)

Property owner's telephone #: 614-575-4824

Location of project: 0 Taylor Rd. Pataskala, Ohio 43063

**Please attach a copy of the project plans and submit with this application (if applicable).**

Describe the work to be completed, **including materials to be used:**  
(i.e. pipe type, fill material, etc...)

Widen Existing Drive  
1. RCP Pipe  
2. Concrete Entrance

The above applicant agrees to adhere to all sections of City of Pataskala Codified Ordinances, Chapter 905, as it relates to work in the right-of-way.

If the proposed construction/installation requires the opening of the pavement, you must provide the following information:

info@stimmelconstruction.com

1. Conditions necessitating opening of pavement: None

2. The opening to be made in the pavement will be \_\_\_\_\_ feet long by \_\_\_\_\_ feet wide and \_\_\_\_\_ feet deep.

3. Pavement will be replaced by (list contractor) \_\_\_\_\_ to the satisfaction of the Director of Public Services.

The applicant hereby agrees to backfill the trench of said opening in compliance with the standards of the Ohio Department of Transportation using construction methods, materials, and workmanship as prescribed in the Construction Materials Specifications (Current Edition) of the Department of Transportation.

*All work requiring workers or vehicles on the pavement or roadway shoulders shall comply with all of the Ohio Manual of Uniform Traffic Control Devices and item 614 (Maintaining Traffic) of the Ohio Department of Transportation construction and material specifications. Failure to comply with this requirement will be cause for immediate suspension of this permit until proper traffic controls have been implemented.*

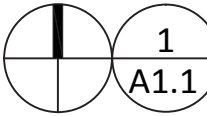
**48 HOURS PRIOR TO EXCAVATION, ALL PERMITTEES MUST NOTIFY THE OHIO UTILITIES PROTECTION SERVICE AT 1-800-362-2764.**

We agree to comply with all of the conditions, restrictions, and regulations of the Department of Transportation and the City of Pataskala.

Stimmel Construction Co.  
Applicant's name (print) Jeff Stimmel

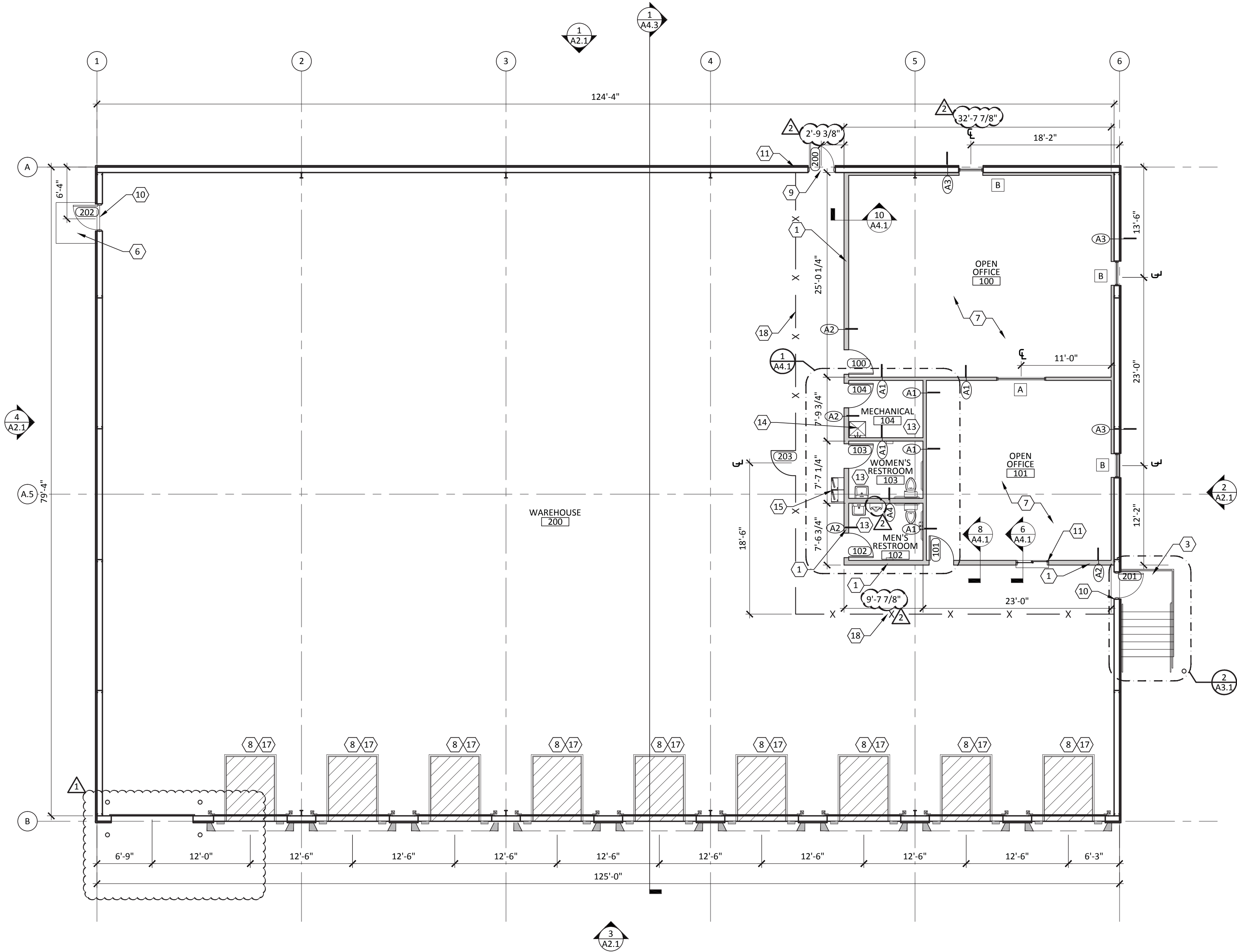
8/22/2016  
Date

[Signature]  
Applicant's signature



## OVERALL FLOOR PLAN

1/8" = 1'-0"



## GENERAL FLOOR PLAN NOTES

- ALL INTERIOR WALLS ARE 20 GAUGE, 3-5/8" METAL STUDS AT 24" O.C. WITH 5/8" GYP. BOARD ON BOTH SIDES OF THE WALL, FROM SLAB TO 6" ABOVE THE HIGHEST ADJACENT CEILING, U.N.O.
- FUR OUT EXTERIOR WALLS IN THE OFFICE AREA WITH 20 GAUGE, 3-5/8" METAL STUDS AT 24" O.C. WITH 5/8" GYP. BOARD, FROM SLAB TO 4" ABOVE HIGHEST ADJACENT CEILING, U.N.O.
- VERIFY ALL UNMARKED WALLS WITH ARCHITECT.
- GENERAL CONTRACTOR TO PROVIDE ALL WALLS INDICATED ON FLOOR PLAN.
- PROVIDE 2x6 WOOD BLOCKING IN PARTITIONS TO SUPPORT ALL CASEWORK, DOOR WALL STOPS, ELECTRICAL AND MECHANICAL DEVICES, AND FIRE EXTINGUISHERS.
- ELECTRICAL CONTRACTOR TO COORDINATE LOCATIONS WITH MECHANICAL EQUIPMENT AND FIRE PROTECTION PRIOR TO STARTING WORK.
- GENERAL CONTRACTOR TO PATCH ALL ROOF PENETRATIONS MADE BY THIS PROJECT WITH ROOFING CONTRACTOR.
- GENERAL CONTRACTOR TO PROVIDE ALL ROOF PATCHING AROUND MECHANICAL EQUIPMENT. VERIFY ROOF WARRANTY REQUIREMENTS WITH OWNER.
- VERIFY ALL OWNER SUPPLIED EQUIPMENT BEFORE ROUGH-INS ARE COMPLETED & GYP. BOARD IS INSTALLED.
- ELECTRICAL CONTRACTOR TO COORDINATE LIGHT FIXTURE LOCATIONS WITH MECHANICAL EQUIPMENT PRIOR TO INSTALLATION.
- FRAMING CONTRACTOR TO COORDINATE FRAMING AROUND HVAC DUCTS, PIPES, CONDUITS AND OTHER ITEMS LOCATED ABOVE THE CEILING.
- THERMAL AND ACOUSTICAL INSULATION IN FLOORS, WALLS AND CEILING TO COMPLY WITH STATE AND LOCAL CODE REQUIREMENTS FOR FLAME SPREAD AND SMOKE DEVELOPMENT RATINGS.
- ALL SHEATHING THAT IS TO RECEIVE FULLY ADHERED ROOF MEMBRANE IS TO BE ATTACHED TO METAL FRAMING BY THE USE OF NON-REVERSING SCREWS. NO NAIL FASTENERS ARE PERMITTED.
- ELECTRICAL CONTRACTOR TO PROVIDE CONDUIT & PULL STRINGS FOR OWNER SUPPLIED COMMUNICATION.
- PROVIDE TACTILE EXIT SIGNAGE AT EACH DOOR TO AN EGRESS STAIRWAY, AN EXIT PASSAGEWAY AND ALL EXIT DISCHARGE POINTS.
- ALL MATERIALS SPECIFIED ARE TO BE INSTALLED IN ACCORDANCE WITH MANUFACTURER'S INSTRUCTIONS AND SPECIFICATIONS. CONTRACTOR IS TO CONSTRUCT THE PROJECT ACCORDING TO THE CONTRACT DOCUMENTS. ANY DEVIATION FROM THE INTENT OF THE CONTRACT DOCUMENTS WITHOUT ARCHITECT OR OWNER APPROVAL ARE AT THE CONTRACTOR'S OWN RISK.
- VERIFY LOCATION OF ALL EQUIPMENT AND VERIFY SIZES, WALL OPENINGS, AND SUPPORT REQUIREMENTS WITH MANUFACTURER. PROVIDE REINFORCEMENT AS REQUIRED BY MANUFACTURER.
- ALL DOORS TO BE LOCATED 4" FROM ADJACENT WALL OR COUNTER, OR CENTERED IN WALL UNLESS OTHERWISE NOTED.
- THE GENERAL CONTRACTOR IS TO VERIFY SIZE, QUANTITY AND LOCATION OF FIRE EXTINGUISHERS W/ LOCAL FIRE MARSHAL PRIOR TO INSTALLATION AND CONFIRM WITH OWNER WHO IS TO PROVIDE THEM.
- DO NOT SCALE OF PLANS, CALL THE ARCHITECT FOR ADDITIONAL DIMENSIONAL INFORMATION IF REQUIRED.
- WRITTEN DIMENSIONS HAVE PRECEDENT OVER SCALED DIMENSIONS IN ALL CASES. G.C. SHALL VERIFY ALL DIMENSIONS AT THE JOB SITE AND NOTIFY THE ARCHITECT OF ANY DISCREPANCIES PRIOR TO STARTING WORK.
- SOUND ATTENUATION BLANKETS TO EXTEND FROM SLAB TO TOP MTL TRACK AND FROM CORNER TO CORNER UNLESS OTHERWISE NOTED. SEE PLAN AND WALL TYPE LEGEND FOR LOCATIONS.
- ALL FLASHING AND SEAMS BETWEEN SHEATHING IN COMPOSITE METAL STUD WALL CONSTRUCTION CONDITIONS TO BE TAPED & SEALED WITH TAPE SEALANT.

## WAREHOUSE FLOOR PLAN CODED NOTES

- PAINT ALL EXPOSED GYPSUM WALLS IN THE WAREHOUSE WITH ONE COAT MINIMUM WATER BASE ACRYLIC LATEX, FLAT WHITE.
- ELECTRIC DISTRIBUTION EQUIPMENT, REFER TO ELECTRICAL DRAWINGS.
- METAL STAIR W/ GALVANIZED RAILING REFER TO 1,2/A3.1.
- GAS METER, VERIFY FINAL LOCATION ON PLUMBING DRAWING.
- ELECTRICAL TRANSFORMER ON CONCRETE PAD, REFER TO CIVIL & ELECTRICAL DRAWINGS
- CONCRETE STOOP SLOPED 1/8" AWAY FROM THE BUILDING, REFER TO CIVIL DRAWINGS. REFER TO DETAIL 2/A4.3.
- PROVIDE 10 MIL VAPOR BARRIER BELOW OFFICE CONCRETE SLAB, REFER TO STRUCTURAL DRAWINGS.
- PROVIDE DOCK LIGHT UNIT AT IN THIS LOCATION.
- PROVIDE ACCESSIBLE TACTILE "EXIT" SIGN AT THIS LOCATION.
- PROVIDE NON-ACCESSIBLE TACTILE "EXIT" SIGN AT THIS LOCATION
- PASS-THRU WINDOW.
- FURNITURE/ EQUIPMENT BY TENANT.
- PROVIDE MOISTURE RESISTANT G.W.B. THROUGHOUT THIS ROOM.
- MOP SINK. PROVIDE 4'-0" HIGH F.R.P. ON WALLS, CAULK ALL CORNERS. F.R.P. TO EXTEND MIN. 24" PAST EDGE OF MOP SINK IN BOTH DIRECTIONS PER ELEVATION 5/A4.1.
- WATER FOUNTAIN
- FROST PROOF HOSE BIBB, REFER TO PLUMBING DRAWINGS.
- INSTALL SAFE-T-SHIELD AT THIS LOCATION. INSTALL PER MANUFACTURER'S STANDARDS.
- 6'-0" HIGH CHAIN LINK FENCE.

## SYMBOL LEGEND

X#	WALL TYPE TAG
X	WINDOW TYPE TAG (SEE SHEET A8.1)
###X	DOOR TYPE TAG (SEE SHEET A8.1)

## WALL LEGEND

=====	EXTERIOR METAL BUILDING WALL
=====	NEW WALL/PARTITION

## TOP OF WALL BRACING LEGEND

SPACING BETWEEN INTERSECTING PERPENDICULAR WALL	TOP TRACK
6'	3 5/8" x 25 GA. TRACK
10'	3 5/8" x 20 GA. TRACK
14'	CS16 x 20 GA. STUD + 3 5/8" x 25 GA. TRACK
18'	CS16 x 14 GA. STUD + 3 5/8" x 25 GA. TRACK
20'	CS18 x 18 GA. STUD + 3 5/8" x 25 GA. TRACK
24'	CS18 x 12 GA. STUD + 3 5/8" x 25 GA. TRACK

## WALL PARTITION SCHEDULE

		DETAIL
A1	5/8" GWB FROM SLAB TO 9'-0" A.F.F. 3-5/8" MTL. STUDS 25 GA. @ 24" O.C. R-13 BATT INSULATION TO 6" A.F.C. 5/8" GWB FROM SLAB TO 6" A.F.C. OFFICE SIDE	3/A4.1
6" A.F.C.		
A2	5/8" GWB FROM SLAB TO DECK. 6" MTL. STUDS 20 GA. @ 24" O.C. R-13 SOUND BATT INSULATION TO 6" A.F.C. 5/8" GWB FROM SLAB TO 6" A.F.C. OFFICE SIDE	8/A4.1 10/A4.1
TO DECK		
A3	PEMB WALL PANEL R-19 WHITE VINYL BACKED INSULATION 5/8" GWB FROM SLAB TO 9'-0" A.F.F. 3-5/8" MTL. STUDS 25 GA. @ 24" O.C. 5/8" GWB FROM SLAB TO 6" A.F.C. OFFICE SIDE	9/A4.1
6" A.F.C.		
A4	5/8" GWB FROM SLAB TO 9'-0" A.F.F. 6" MTL. STUDS 25 GA. @ 24" O.C. R-13 SOUND BATT INSULATION TO 6" A.F.C. 5/8" GWB FROM SLAB TO 6" A.F.C. OFFICE SIDE	3/A4.1
6" A.F.C.		

### NOTES:

- FIELD VERIFY ALL DIMENSIONS. DIMENSIONS ARE TO THE FACE OF METAL FINISH FRAMING UNLESS NOTED OTHERWISE.
- PROVIDE VERTICAL CONTROL JOINTS AT 30'-0" O.C. MAX. SEE DETAIL 11/A8.3.
- ALL PARTITIONS THAT ARE TO BE BUILT TO DECK SHALL INCLUDE ANY STRUCTURE (JOISTS, BEAMS, ETC.) THAT MAY NEED TO BE FRAMED OUT IN ORDER TO PROVIDE DRYWALL TO DECK. ALL PENETRATIONS AND FLUTES IN DECK SHALL BE SEALED AS REQUIRED.
- PROVIDE BRACING PER MANUFACTURER'S RECOMMENDATIONS IN ALL PARTITIONS THAT DO NOT EXTEND FULL HEIGHT TO THE UNDERSIDE OF THE STRUCTURE ABOVE.
- PROVIDE A GALVANIZED STEEL SHEET DEEP LEG DEFLECTION TRACK, 'SLP-TRK' BY SLIPTRACK SYSTEMS, INC. OR EQUAL AT ALL FRAMING THAT EXTENDS FROM THE SLAB TO THE UNDERSIDE OF THE STRUCTURE ABOVE. SECURE DEFLECTION TRACK TO STRUCTURE WITH FASTENERS AT 2'-0" O.C. MAXIMUM. DO NOT SECURE TRACK TO VERTICAL STUDS. PROVIDE MINIMUM OF 7/8" TO TOP OF METAL STUD. REFER TO SPECIFICATIONS.
- ALL BOTTOM TRACK RUNNERS ARE TO BE HELD IN PLACE WITH FASTENERS AT 2'-0" O.C. MAX WITH HILTI POWER DRIVER ANCHOR ICBO #2388, .0145" DIAMETER SHANK WITH 1 1/8" MINIMUM PENETRATION OR EQUAL.
- PROVIDE ACOUSTICAL SEALANT AT THE INTERSECTION OF ALL GYPSUM BOARD TO DECK DECK OR CONCRETE SLAB CONDITIONS. HOLD GYPSUM BOARD 3/8" OFF OF STRUCTURE FOR SEALANT.
- METAL STUD NON-STRUCTURAL WALL SYSTEMS ARE TO FOLLOW ASTM C754 & ASTM C645.
- TAPE, MUD, AND SAND GYPSUM BOARD SMOOTH READY FOR TENANT FINISHES AT INTERIOR FACE OF ALL GYPSUM BOARD SURFACES. PROVIDE ASTM C840 LEVEL 4 FINISH UNLESS NOTED OTHERWISE.
- WATER RESISTANT GYPSUM BOARD TO BE 5/8" USG SHEETROCK BRAND MOLD TOUGH GYPSUM PANELS OR EQUAL COMPLYING WITH ASTM C1178, C1288, OR C1325. PROVIDE AT ALL TOILET ROOM WALLS, PLUMBING CHASE WALLS, JANITOR CLOSETS, MECHANICAL ROOMS, AND BUILDING SERVICES ROOM.
- TILE BACKER BOARD TO BE 5/8" 'DENS-SHIELD TILE BACKER' BY GEORGIA PACIFIC OR EQUAL.
- ALL PLYWOOD BLOCKING IS TO BE FIRE RETARDANT TREATED.
- PROVIDE 2 X 6 FIRE RETARDANT TREATED WOOD BLOCKING IN PARTITIONS TO SUPPORT URINALS, DRINKING FOUNTAINS, MOP SINKS, TOILET ACCESSORIES, HAND RAILS, AND GRAB BARS.

## DOCK EQUIPMENT

- ALL DOCKS TO RECEIVE THE FOLLOWING EQUIPMENT, U.N.O.:
- (DOCK EQUIPMENT BY RITE HITE)
- 7'-0" X 8'-0" 30,000 LB CAPACITY AIR OPERATED, 4 1/2" LAMINATED BUMPERS.
  - DOCK SHELTER: GAPMASTER SHELTER TO FIT 9'-0" x 10'-0" DOCK DOORS
  - DOCK LIGHT: SWING ARM DOCK LIGHTS - LED FIXTURES (NOT JUST LED BULBS)
  - DOCK DOOR TRACK GUARDS.
  - SAFE-T-SHIELDS
  - DOCK LOCK



MARK HEADLEE  
LICENSE #10425 EXPIRES: 12.31.19

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TS TECH CROSS DOCK  
6630 TAYLOR RD SW.  
PATASKALA, OHIO 43068  
FOR  
HUMBLE CONSTRUCTION  
3441 MORSE RD., COLUMBUS, OHIO 43231

PROJECT NUMBER:  
HUM132

ISSUE	DATE
PRELIMINARY	JAN. 30, 2019
PERMIT	FEB. 11, 2019
REVISION 1	FEB. 27, 2019
REVISION 2	MAR. 27, 2019

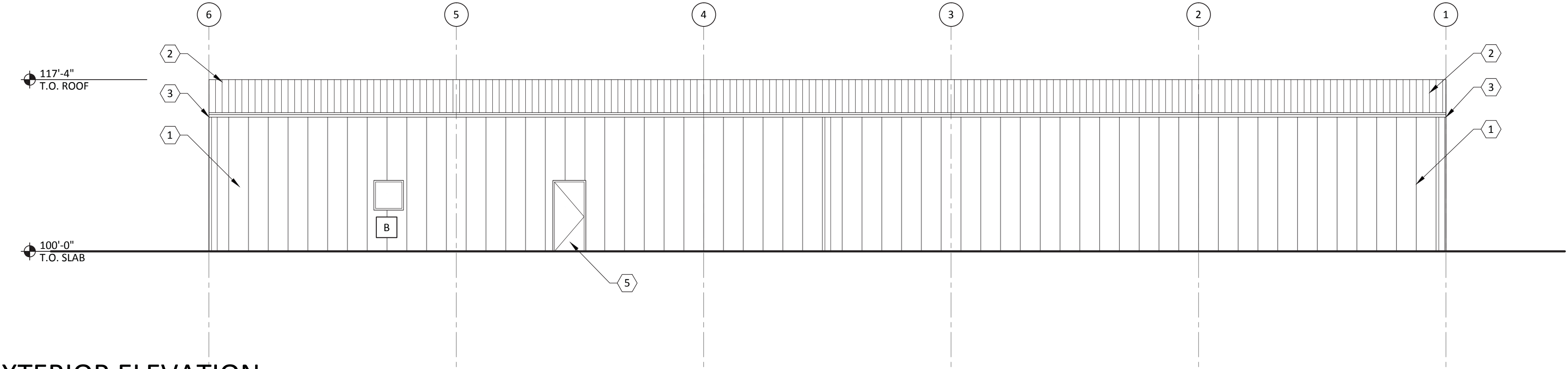
OVERALL FLOOR PLAN

SHEET:

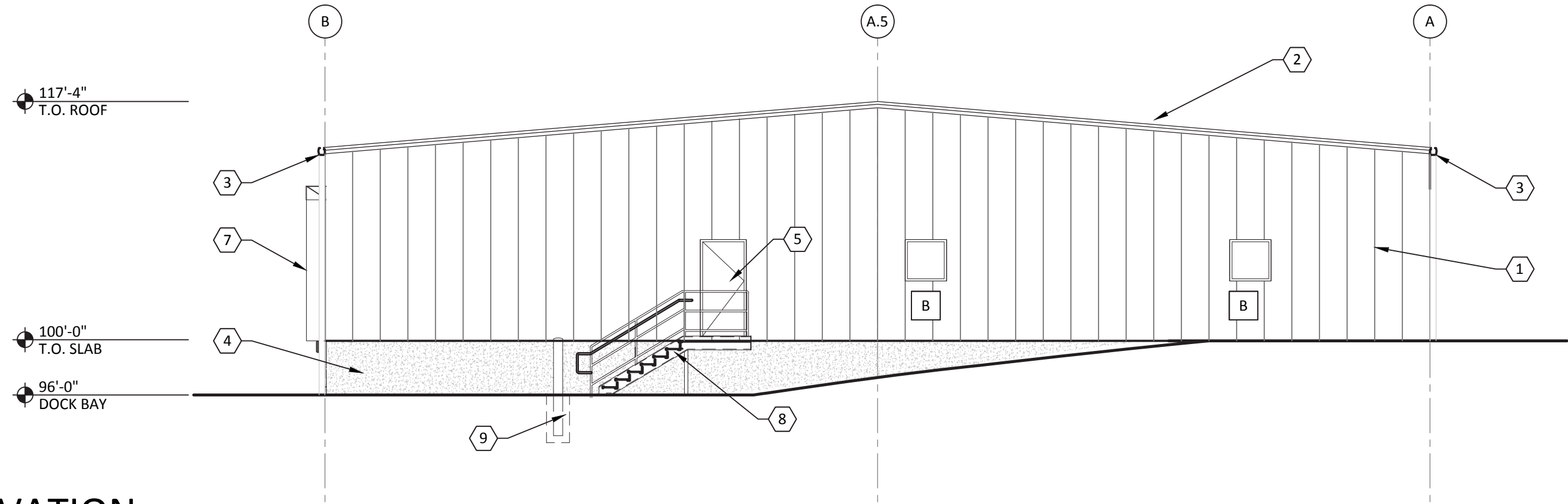
A1.1

red  
architecture + planning  
174 e long st.  
suite 300  
columbus, ohio 43215  
tel: 614.487.8770  
fax: 614.487.8770

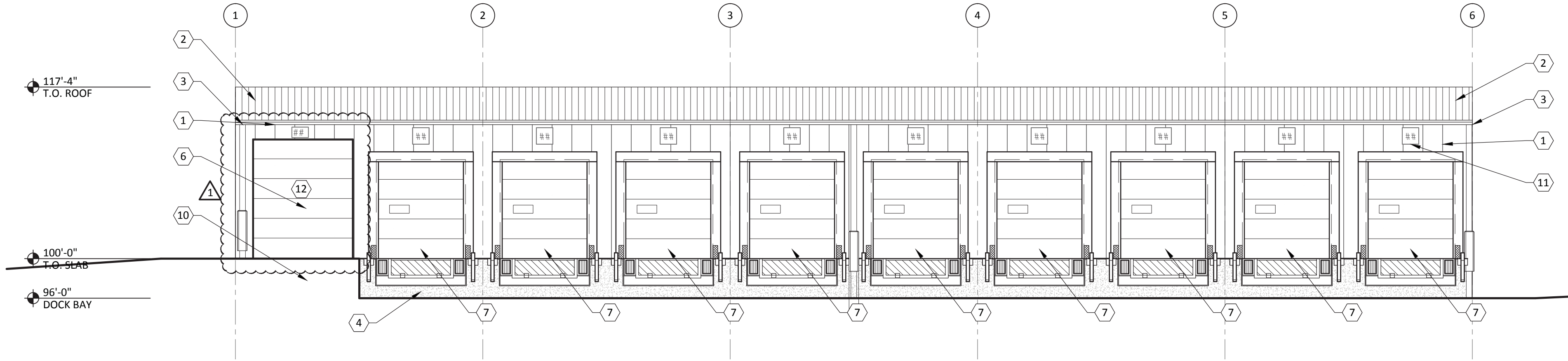
3/27/2019 7:55 AM



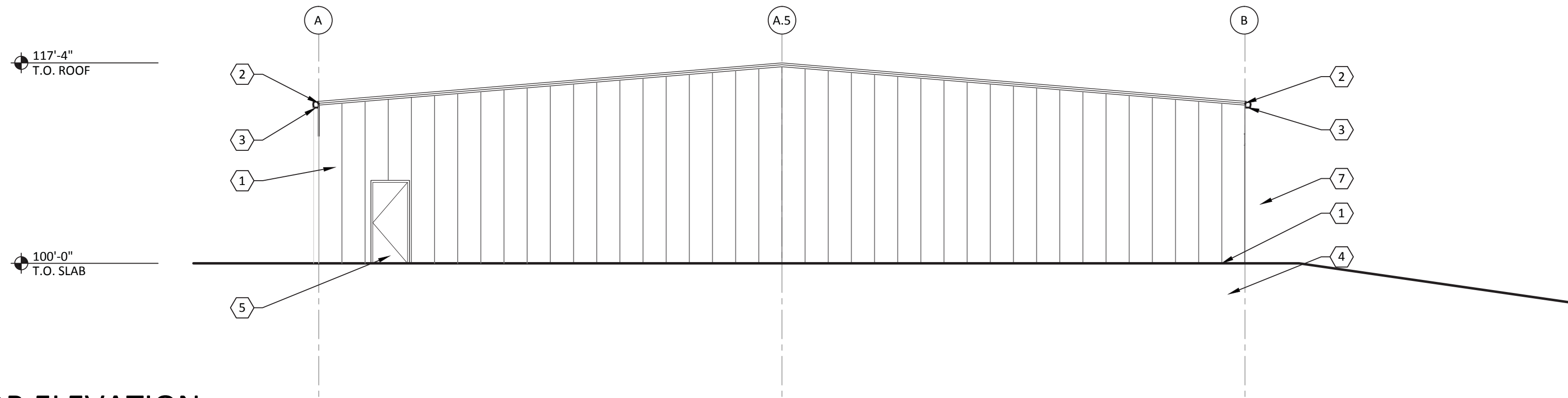
1  
A2.1  
EXTERIOR ELEVATION  
1/8" = 1'-0"



2  
A2.1  
EXTERIOR ELEVATION  
1/8" = 1'-0"



3  
A2.1  
EXTERIOR ELEVATION  
1/8" = 1'-0"



4  
A2.1  
EXTERIOR ELEVATION  
1/8" = 1'-0"

## CODED NOTES

1. PEMB WALL PANEL.
2. PEMB ROOF PANEL.
3. PRE-FINISHED METAL GUTTER AND DOWNSPOUTS.
4. CONCRETE FOUNDATION.
5. INSULATED HOLLOW METAL DOOR AND FRAME. REFER TO DOOR SCHEDULE.
6. INSULATED SECTIONAL OVERHEAD DOOR.
7. DOCK DOOR AND SEAL. REFER TO SHEET A4.2 FOR DOCK DOOR DETAILS.
8. STEEL STAIR. REFER TO SHEET A3.1 FOR DETAILS.
9. BOLLARD. REFER TO SHEET A3.1 FOR DETAILS.
10. DRIVE-IN DOOR RAMP.
11. COORDINATE DOCK DOORS WITH TENANT.
12. 10'-0" x 12'-0" OVERHEAD DOOR.

red  
architecture + planning  
174 e longst.  
suite 300  
columbus, ohio 43215  
tel: 614.487.8770  
fax: 614.487.8777



MARK HEADLEE  
LICENSE #10425 EXPIRES: 12.31.19

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ARCHITECTURE + PLANNING LLC  
AND MAY NOT BE USED,  
DUPLICATED OR ALTERED  
WITHOUT THE WRITTEN CONSENT  
OF THE ARCHITECT.

TS TECH CROSS DOCK  
6630 TAYLOR RD SW.  
PATASKALA, OHIO 43068  
FOR  
HUMBLE CONSTRUCTION  
3441 MORSE RD., COLUMBUS, OHIO 43231

PROJECT NUMBER:  
HUM132

ISSUE	DATE
PRELIMINARY	JAN. 30, 2019
PERMIT	FEB. 11, 2019
REVISION 1	FEB. 27, 2019
REVISION 2	MAR. 27, 2019

EXTERIOR ELEVATIONS

SHEET:

A2.1

# TS TECH AMERICA CROSS DOCK IMPROVEMENT

## 6630 TAYLOR ROAD SW PATASKALA, LICKING COUNTY, OHIO 2019

### FRANKLIN COUNTY SURVEY CONTROL REFERENCE MONUMENTS

Designation: FRANK48  
Station is a Stainless Steel Rod driven to a depth of 12 Feet on the North side of East Broad Street, 0.9 miles East of Waggoner Road to #8400 East Broad Street, 26.0-Feet North of the North edge of the paved berm, 66-Feet West of the centerline of the drive to the plant, 13-Feet Southeast of the TS Tech North American sign, 23.4-Feet Southeast of a power pole, access through aluminum access cover. see "Frank148" for Azimuth.  
Northing US Survey Ft: 723892.370 Easting US Survey Ft: 1891258.350

Designation: FRANK148  
Aluminum Plug in Concrete Monument at #8400 East Broad Street, 0.3 miles along the entrance drive to Cordinal Industries Plant, 42.4-Feet East of the East edge of pavement, 93.7-Feet Southeast of the base of a metal light pole on the West side of the road, 70.1-Feet Southwest of a manhole, 36.9-Feet Southeast of the top center of the East end of a 4-Feet concrete culvert pipe, 1-inch below the ground. Disk is stamped "Frank48 AZ MK 1987".  
Northing US Survey Ft: 725256.200 Easting US Survey Ft: 1891670.440

### BENCHMARKS

#### BM#1

Iron Pin set and capped with "CASSELL S-6378", approximately 92.5-Feet West of the existing entrance driveway of TS Tech USA & TS Tech America (8400 East Broad Street), 269.5-Feet Southeast from the Southeast corner of the existing pump house building, located in the Southwest area of the existing large pond.

NAVD 88 Elev. = 1029.53

#### BM#2

Center Point of Existing Catchbasin located in the Right-of-Way of Taylor Road at the address 6747 Taylor Road Southwest, approximately 41.8-Feet Northeast of the existing driveway of TS Tech USA.

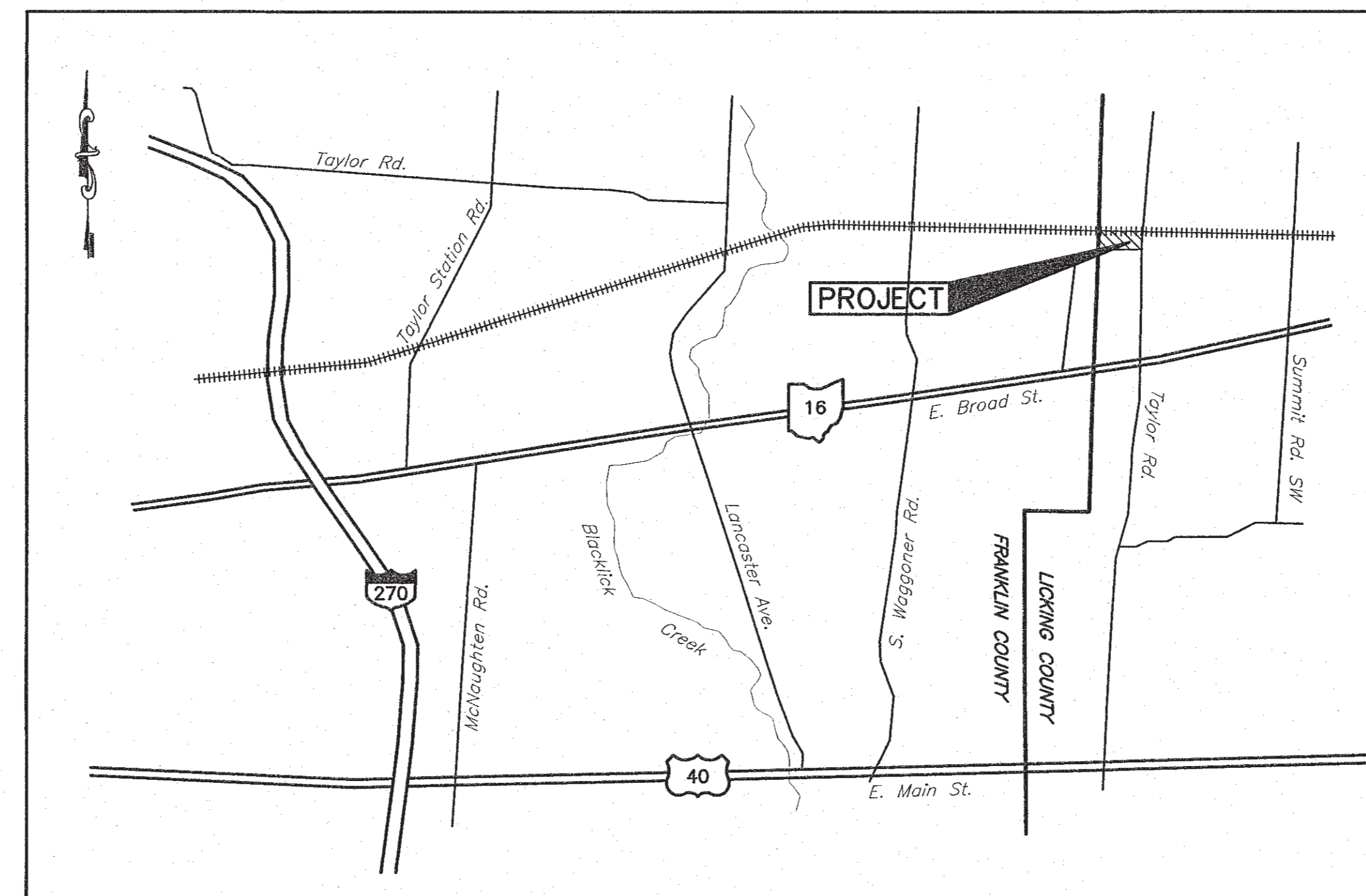
Northing: 727199.5355 Easting: 1893604.6046

NAVD 88 Elev. = 1050.73

### SOUTHWEST LICKING COMMUNITY WATER & SEWER DISTRICT STANDARD CONSTRUCTION DRAWINGS

The Standard Construction Drawings listed on these plans shall be considered a part thereof.

WATER	SANITARY	ODOT
W-1	SoS-9	CB-1.1
W-2	SoS-10	CB-1.2
W-3	SoS-16	HW-2.1
W-4	SoS-18	
W-6		
W-7		
W-12		
W-14		
W-18		
W-20		
W-21		
W-30		
W-38		



### LOCATION MAP

Not To Scale

### SHEET INDEX

1	TITLE SHEET
2	NOTES & DETAILS
3	NOTES & DETAILS
4	EXISTING SITE CONDITIONS
5	OVERALL SITE PLAN
6	SITE PLAN
7	PAVING EXHIBIT
8	SITE UTILITIES PLAN
9	SITE GRADING PLAN
10	STORM & WATER PROFILES
11	WATER LINE DETAIL DRAWINGS
12	WATER LINE DETAIL DRAWINGS
13	DETAILS / DESIGN DRAWINGS
14	EROSION AND SEDIMENT CONTROL PLAN
15	EROSION & SEDIMENT CONTROL NOTES/DETAILS
16	TREE REMOVAL / PRESERVATION PLAN
17	LANDSCAPING PLAN

### NOTE:

Proposed Building: 10,000 GSF

Proposed Total: 10,000 GSF

### SITE DATA:

Parcel ID: 063-140916-00.000

Street Address: 6630 Taylor Rd. SW, Pataskala, OH  
Existing Zoning District: M-1, Manufacturing

Total Site Area: 21.01 Acres  
Pre-Developed Pervious Area: 19.60 Ac.  
Pre-Developed Impervious Area: 1.40 Ac.  
Post-Developed Pervious Area: 18.50 Ac.  
Post-Developed Impervious Area: 2.51 Ac.  
Total Disturbed Area: 3.12 Ac.

Prop. Building: 10,000 S.F.  
Prop. Parking: 5,236 S.F.  
Prop. Driveways: 30,998 S.F.

### Flood Note:

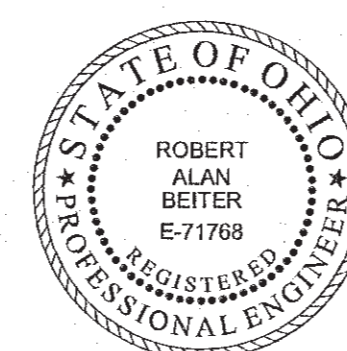
The subject site lies within the limits of Flood Zone "X" as shown on the Flood Insurance Rate Map for Licking County, Ohio.

Map Numbers:  
39089C0407H, May 02, 2007

THE SIGNATURES BELOW SIGNIFY ONLY CONCURRENCE WITH THE GENERAL PURPOSES AND GENERAL LOCATION OF THE PROJECT. DETAILS REMAIN THE RESPONSIBILITY OF THE ENGINEER PREPARING THE PLANS.

APPROVED BY: CITY OF PATASKALA PUBLIC SERVICE DIRECTOR DATE

APPROVED BY: SOUTHWEST LICKING COMMUNITY WATER & SEWER DISTRICT DATE



REGISTERED ENGINEER

NO. E-71768

DATE 05/21/19

PLAN PREPARED BY:

CROSSING  
WATERS  
ENGINEERING, INC.

P.O. Box 27  
260 S. Main St., Suite A  
Sugar Grove, Ohio 43155  
(740) 740-0290  
www.crossingwaterseng.com

NO.	REVISION	BY	DATE	RECORD DRA
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NOTICE TO THE CITY

PRE-CONSTRUCTION VIDEO

WORK HOURS

### SAFETY REQUIREMENTS

### EXISTING UTILITIES

### REPLACEMENT OF DRAIN TILES AND STORM SEWERS

MAINTAIN DRAINAGE

## TRAFFIC CONTROL DEVICES

INGRESS AND EGRESS SHALL BE MAINTAINED TO PUBLIC AND PRIVATE PROPERTY AT ALL TIMES.

### MAINTENANCE OF TRAFFIC

### NON-RUBBER TIRED VEHICLES

### RESTORATION

## MONUMENTS

RECORD DRAWINGS

THE FOLLOWING IS TO BE SUBMITTED TO THE CITY ENGINEER.

-CD WITH PDF OF "RECORD DRAWINGS"



NOTES &amp; DETAILS

NO.	REVISION	BY	DATE	REVISION DRAWING
			BY	DRAWN BY: RAB
			DATE	SCALE: AS NOTED
			CONTRACTOR	CHECKED BY: JAS
			CONSTRUCTED	DATE: 05/21/2019
			NOTES	APPROVED BY: RAB
				SHEET: 3 OF 17



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TS Tech USA Corp.  
063-140916-00.000  
INST#201503300005792

Taylor Road Investments,  
LLC  
INST#200602140004346



CROSSING WATERS ENGINEERING, INC. P.O. BOX 27, 260 S. MAIN ST., SUITE A SUGAR GROVE, OHIO 43155 (740) 746-0250	
TSTECH AMERICA CROSS DOCK IMPROVEMENT 6630 TAYLOR ROAD SW PATASKALA, LICKING COUNTY, OHIO EXISTING SITE CONDITIONS	
CONTRACT	JOB NO.: CW-18016
DRAWN BY: RAB	SCALE: 1" = 40'
CHECKED BY: JAS	DATE: 05/21/2019
APPROVED BY: RAB	SHEET: 4 OF 17



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(740) 746-0250  
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NO.	REVISION	BY	DATE	BY	RECORD	DRAWING



TS Tech USA Corp.  
063-140916-00.000  
INST#201503300005792

Taylor Road Investments,  
LLC  
INST#200602140004346



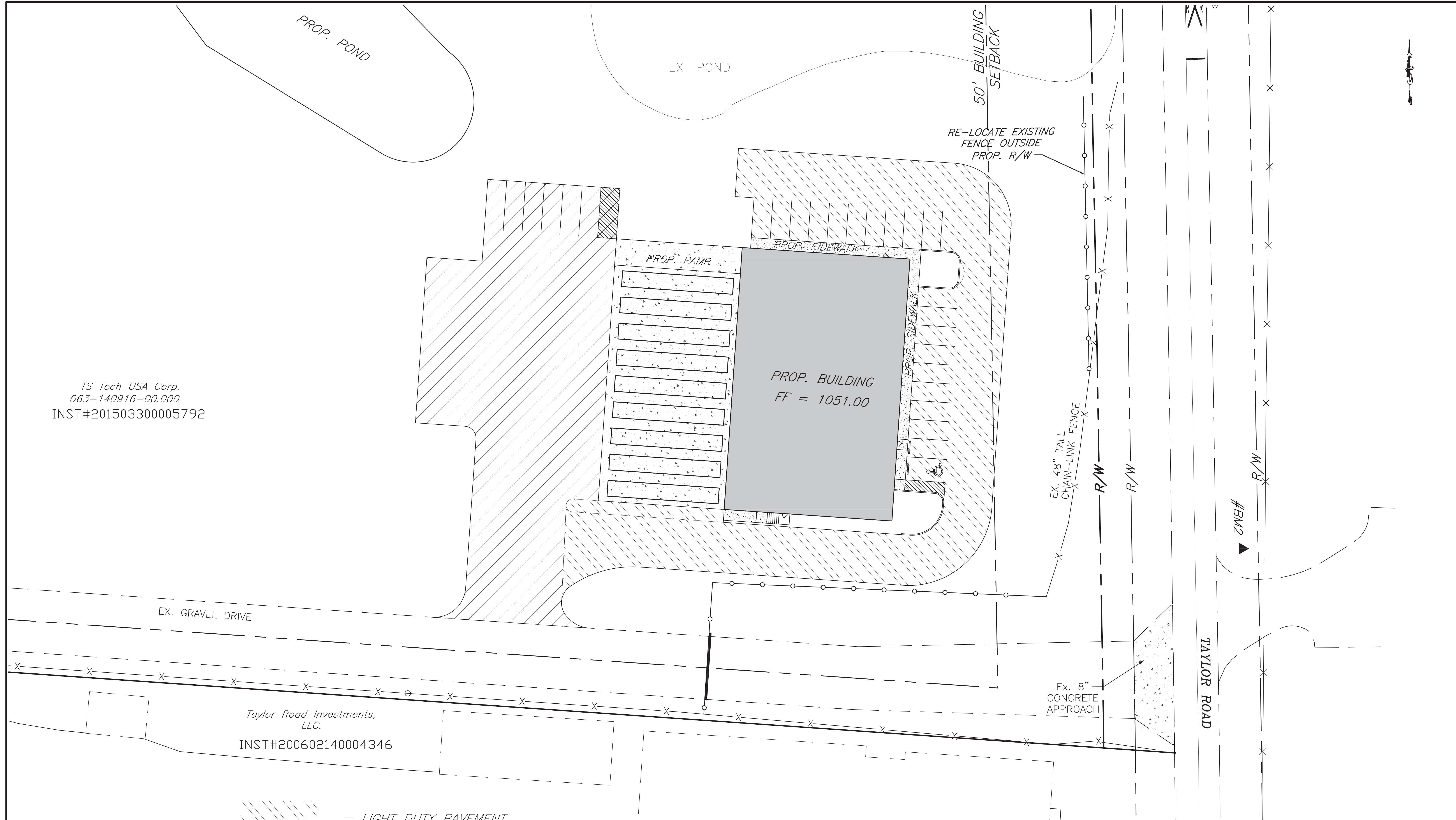
CROSSING WATERS ENGINEERING, INC. P.O. BOX 27, 260 S. MAIN ST., SUITE A SUGAR GROVE, OHIO 43155 (740) 746-0250	
TSTECH AMERICA CROSS DOCK IMPROVEMENT 6630 TAYLOR ROAD SW PATASKALA, LICKING COUNTY, OHIO OVERALL SITE PLAN	
CONTRACT	JOB NO.: CW-18016
DRAWN BY: RAB	SCALE: 1" = 40'
CHECKED BY: JAS	DATE: 05/21/2019
APPROVED BY: RAB	SHEET: 5 OF 17



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INST#201503300005792

EX. GRAVEL DRIVE

Taylor Road Investments,  
LLC.  
INST#200602140004346

- = LIGHT DUTY PAVEMENT
- = HEAVY DUTY PAVEMENT
- = 4" CONCRETE
- = HEAVY DUTY CONCRETE PAVEMENT

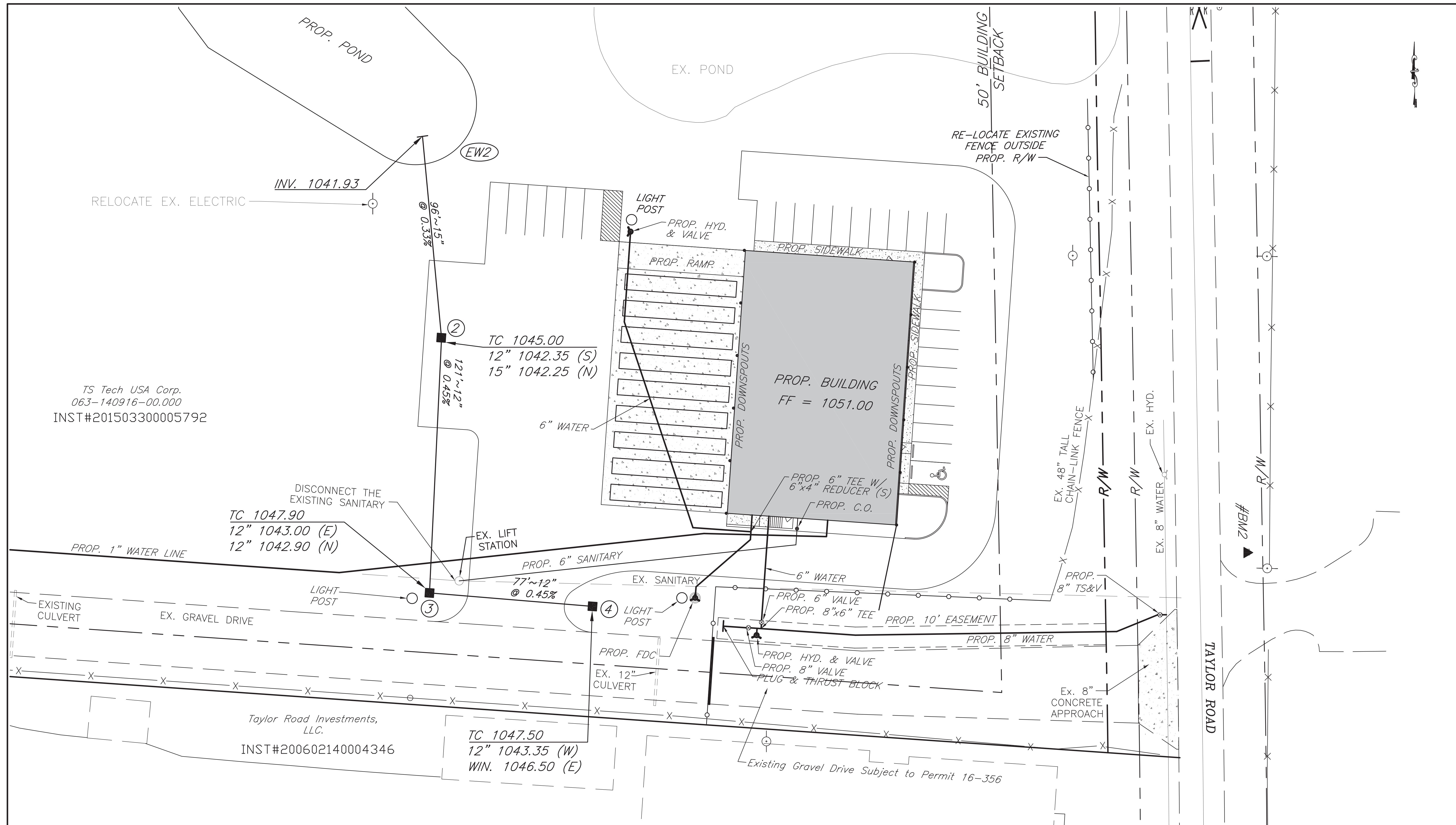


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NO.	REVISION	BY	DATE	RECORD	DRAWING

CROSSING WATERS ENGINEERING, INC. P.O. BOX 27, 260 S. MAIN ST., SUITE A SUGAR GROVE, OHIO 43155 (740) 746-0250	
TSTECH AMERICA CROSS DOCK IMPROVEMENT 6630 TAYLOR ROAD SW PATASKALA, LICKING COUNTY, OHIO PAVING EXHIBIT	
CONTRACT	JOB NO.: CW-18016
DRAWN BY: RAB	SCALE: 1" = 20'
CHECKED BY: JAS	DATE: 05/21/2019
APPROVED BY: RAB	SHEET: 7 OF 17



TS Tech USA Corp.  
063-140916-00.000  
INST#201503300005792

ALL DOWNSPOUTS LOCATED WITHIN THE TRUCK DOCK AREA SHALL OUTLET WITH SPLASH BLOCKS ONTO PAVEMENT.

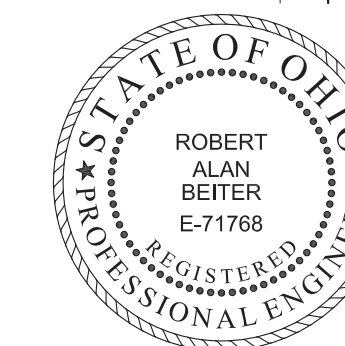
A 6" AMES 4000SS RPZ BACKFLOW PREVENTER & METER SHALL BE INSTALLED INSIDE THE PROPOSED BUILDING.

THE EXISTING WATER SERVICE FOR THIS PROPERTY NEEDS TO BE ABANDONED AT THE 8" WATER MAIN. THE EXISTING WATER SERVICE SADDLE SHALL BE REMOVED AND REPLACED WITH A SMITH BLAIR 261 FULL RUBBER STAINLESS REPAIR BAND OR EQUIVALENT. (FIELD LOCATE)

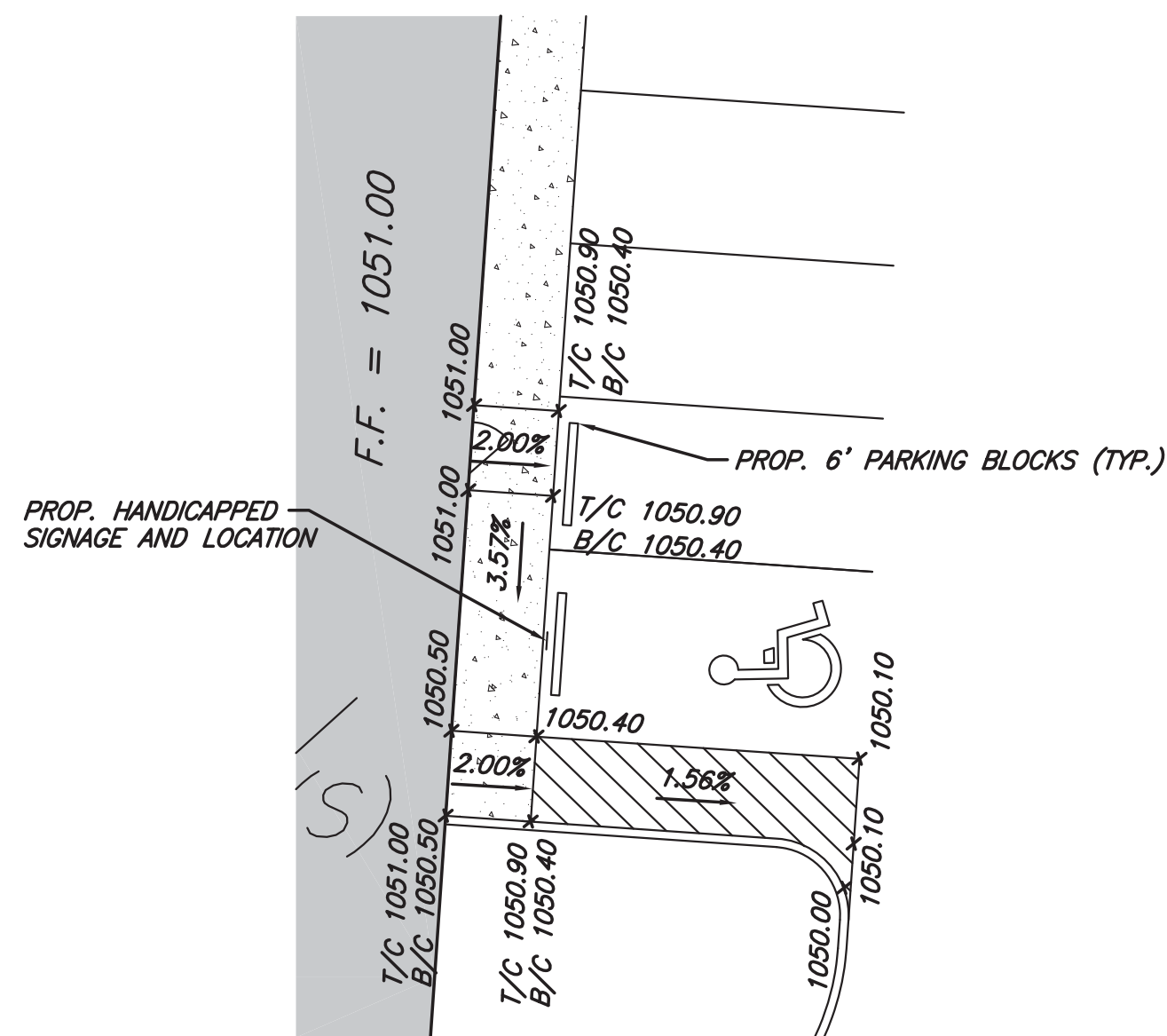
NOTE: Existing Gravel Driveway is subject to City of Pataskala Permit No. 16-356, and the application herein is intentionally submitted without modification or amendment thereto. Applicant reserves the right to seek and apply for separate approval, modification, or variance thereof

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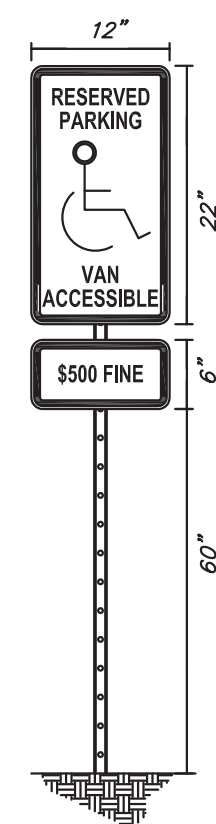


CROSSING WATERS ENGINEERING, INC. P.O. BOX 27, 260 S. MAIN ST., SUITE A SUGAR GROVE, OHIO 43155 (740) 746-0250	
TSTECH AMERICA CROSS DOCK IMPROVEMENT	
6630 TAYLOR ROAD SW PATASKALA, LICKING COUNTY, OHIO	
SITE UTILITIES PLAN	
CONTRACT	JOB NO.: CW-18016
DRAWN BY: RAB	SCALE: 1" = 20'
CHECKED BY: JAS	DATE: 05/21/2019
APPROVED BY: RAB	SHEET: 8 OF 17



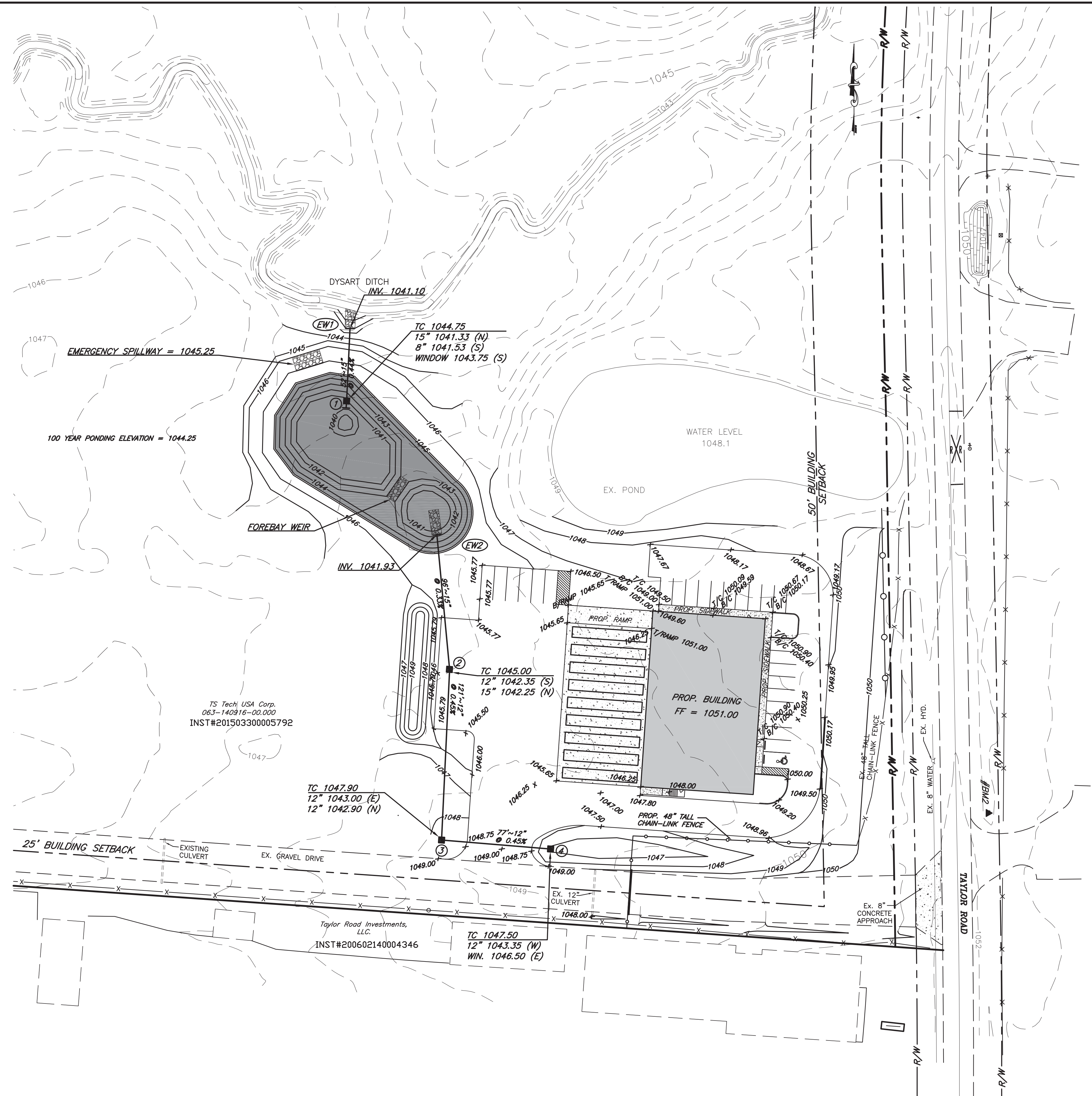
ADA PARKING SPACES AND RAMPS

SCALE = 1" = 10'



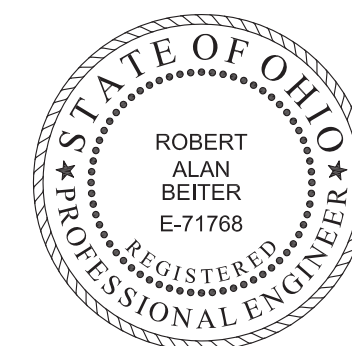
HANDICAPPED SIGN DETAIL  
Not to Scale

ALL PROPOSED ELEVATIONS SHALL  
BE EDGE OF PAVEMENT, UNLESS  
OTHERWISE NOTED.



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INST#201503300005792

Taylor Road Investments,  
LLC.  
INST#200602140004346



CROSSING WATERS ENGINEERING, INC.	
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TSTECH AMERICA CROSS DOCK IMPROVEMENT	
6630 TAYLOR ROAD SW PATASKALA, LICKING COUNTY, OHIO	
SITE GRADING PLAN	
CONTRACT	JOB NO.: CW-18016
DRAWN BY: RAB	SCALE: 1" = 40'
CHECKED BY: JAS	DATE: 05/21/2019
APPROVED BY: RAB	SHEET: 9 OF 17

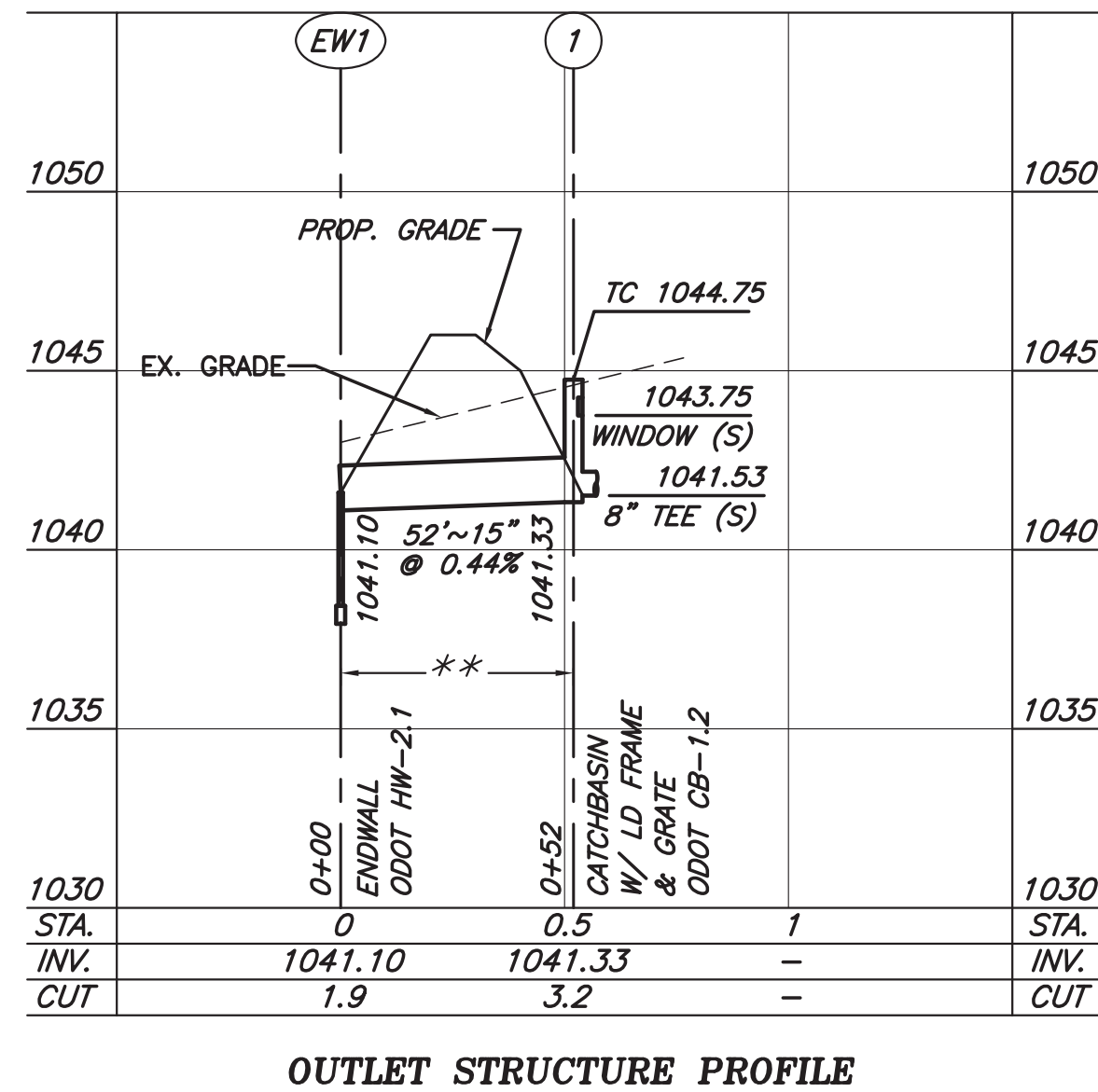
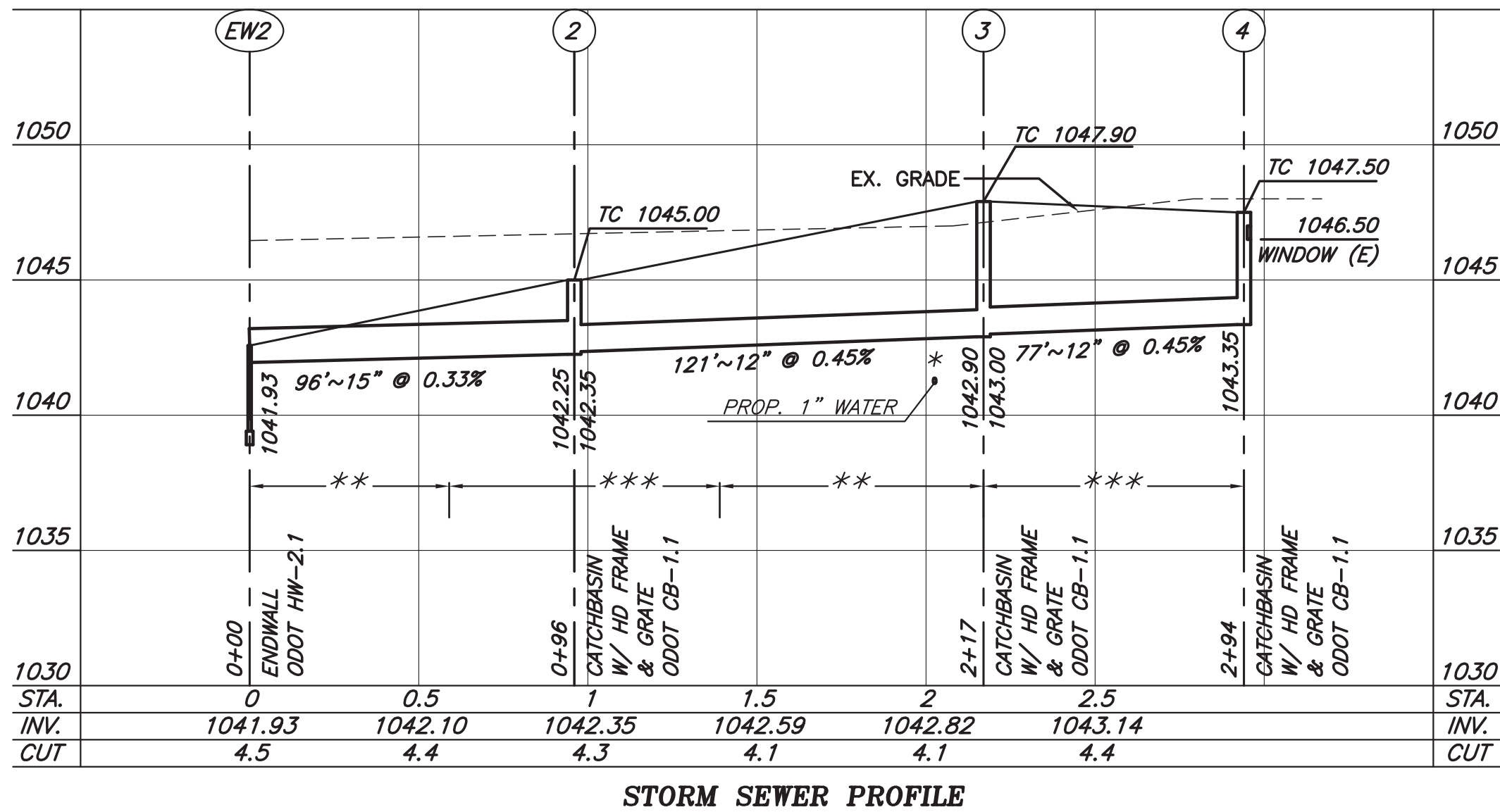
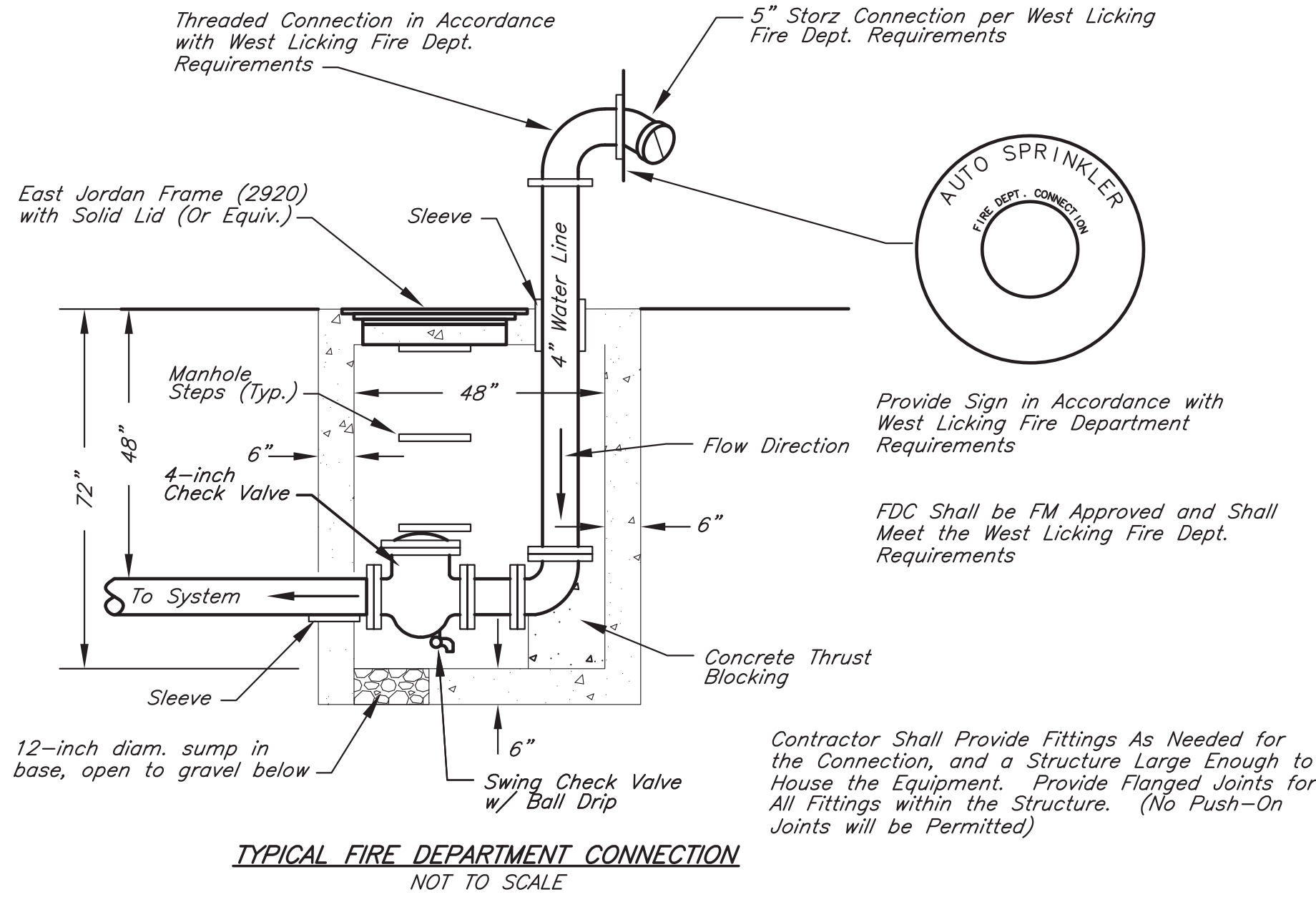
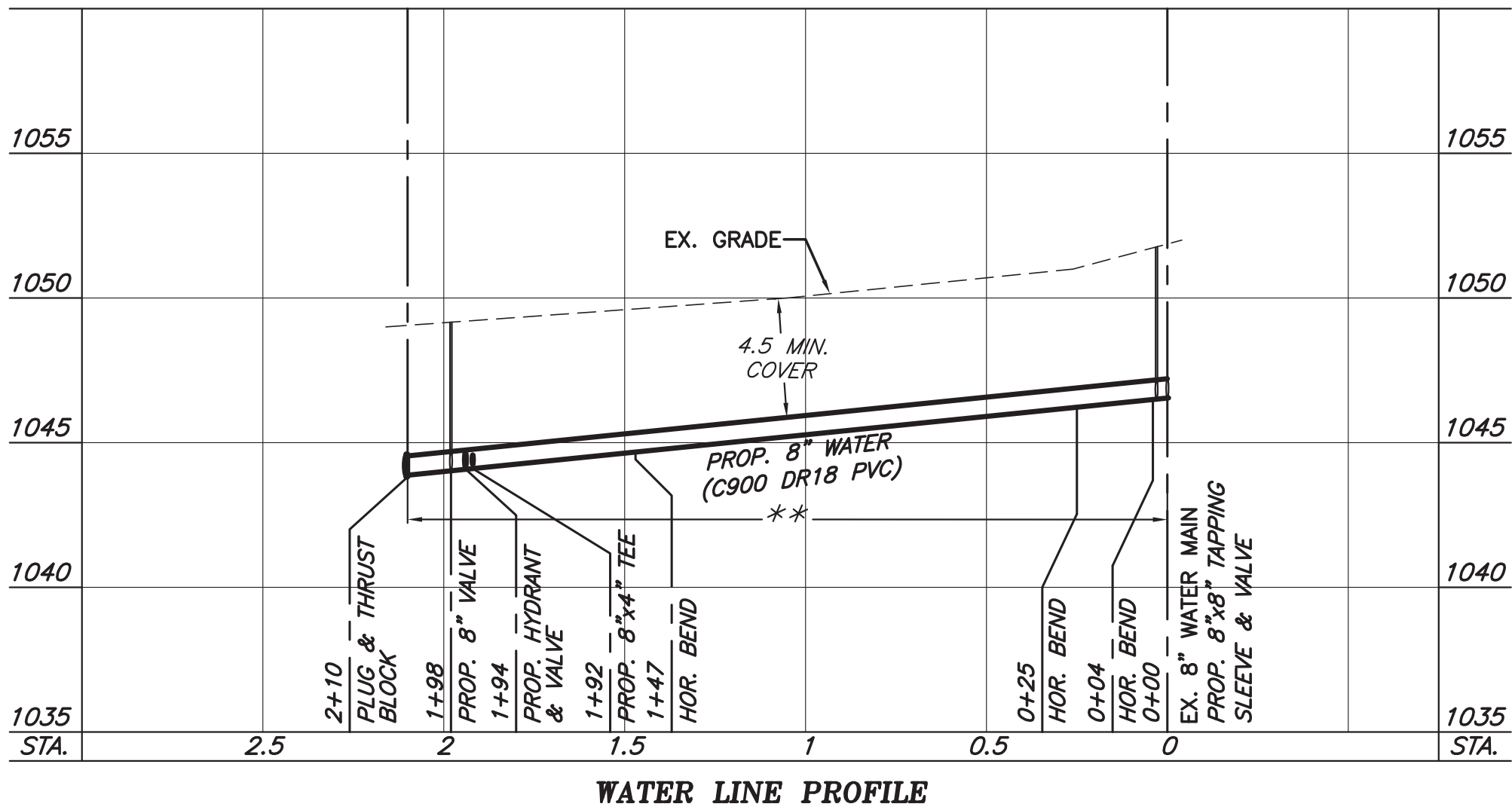
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\* MAINTAIN A MINIMUM OF 18" VERTICAL CLEARANCE.  
\*\* COMPACTED BACKFILL AT LEAST EQUAL IN DENSITY TO ADJACENT UNDISTURBED SOIL.  
\*\*\* COMPACTED GRANULAR BACKFILL.

NO PERSON SHALL DISCHARGE OR CAUSE TO BE DISCHARGED ANY STORM WATER, SURFACE WATER, GROUND WATER, ROOF RUNOFF, SUBSURFACE DRAINAGE, COOLING WATER OR UNPOLLUTED INDUSTRIAL PROCESS WATERS TO ANY SANITARY SEWER LOCATED WITHIN PATASKALA, OHIO, OR WITHIN ANY AREA UNDER THE JURISDICTION OF SAID CITY.

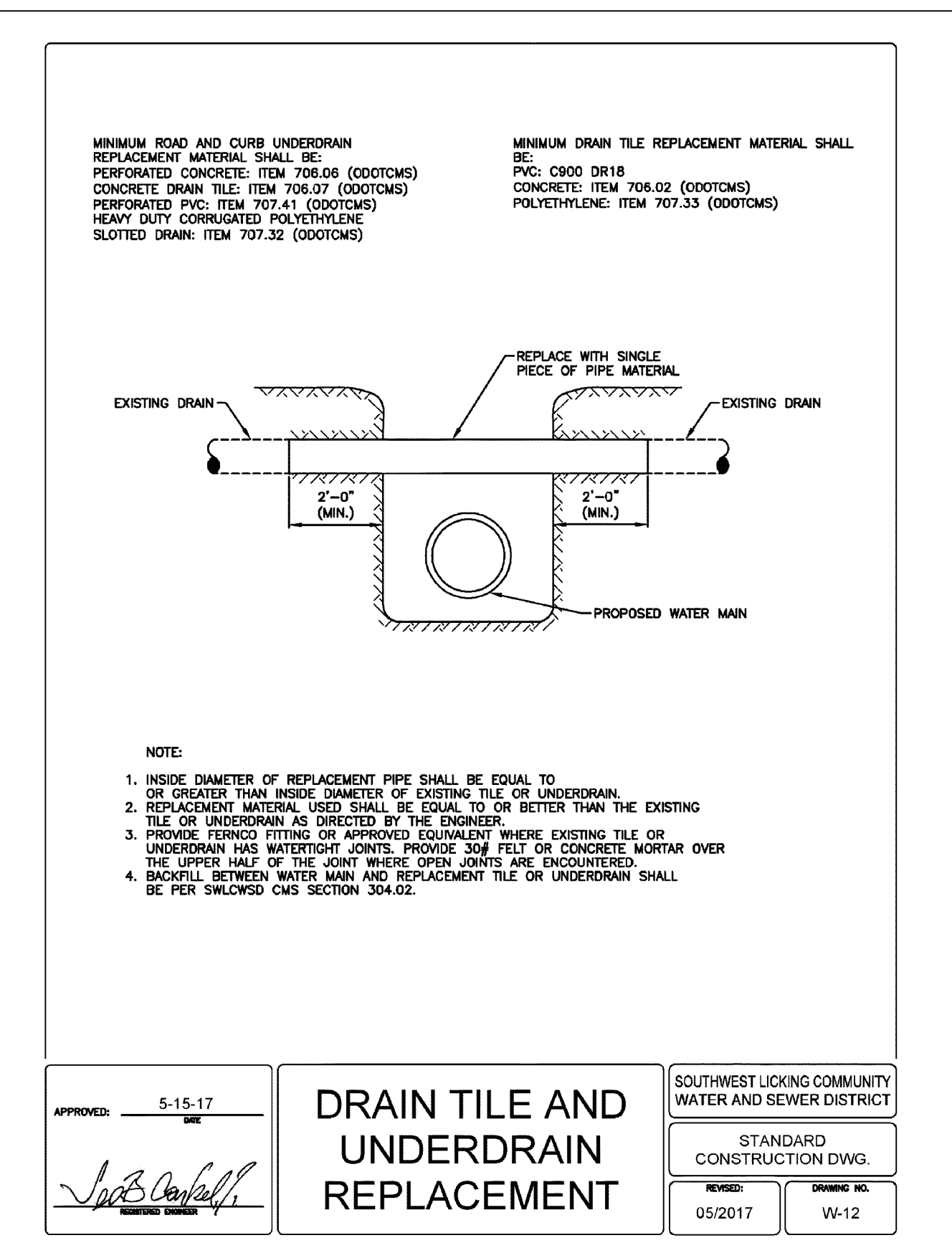
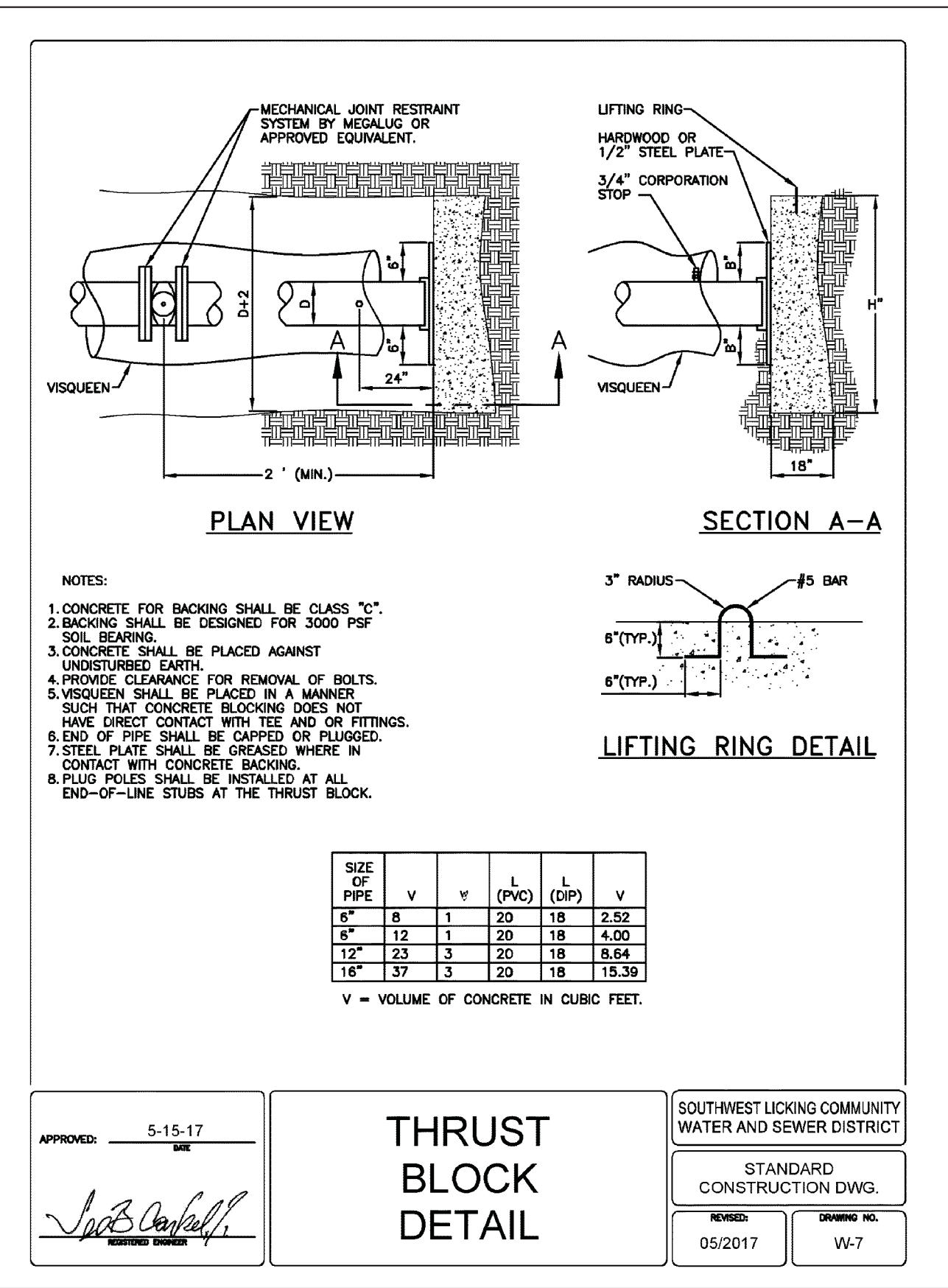
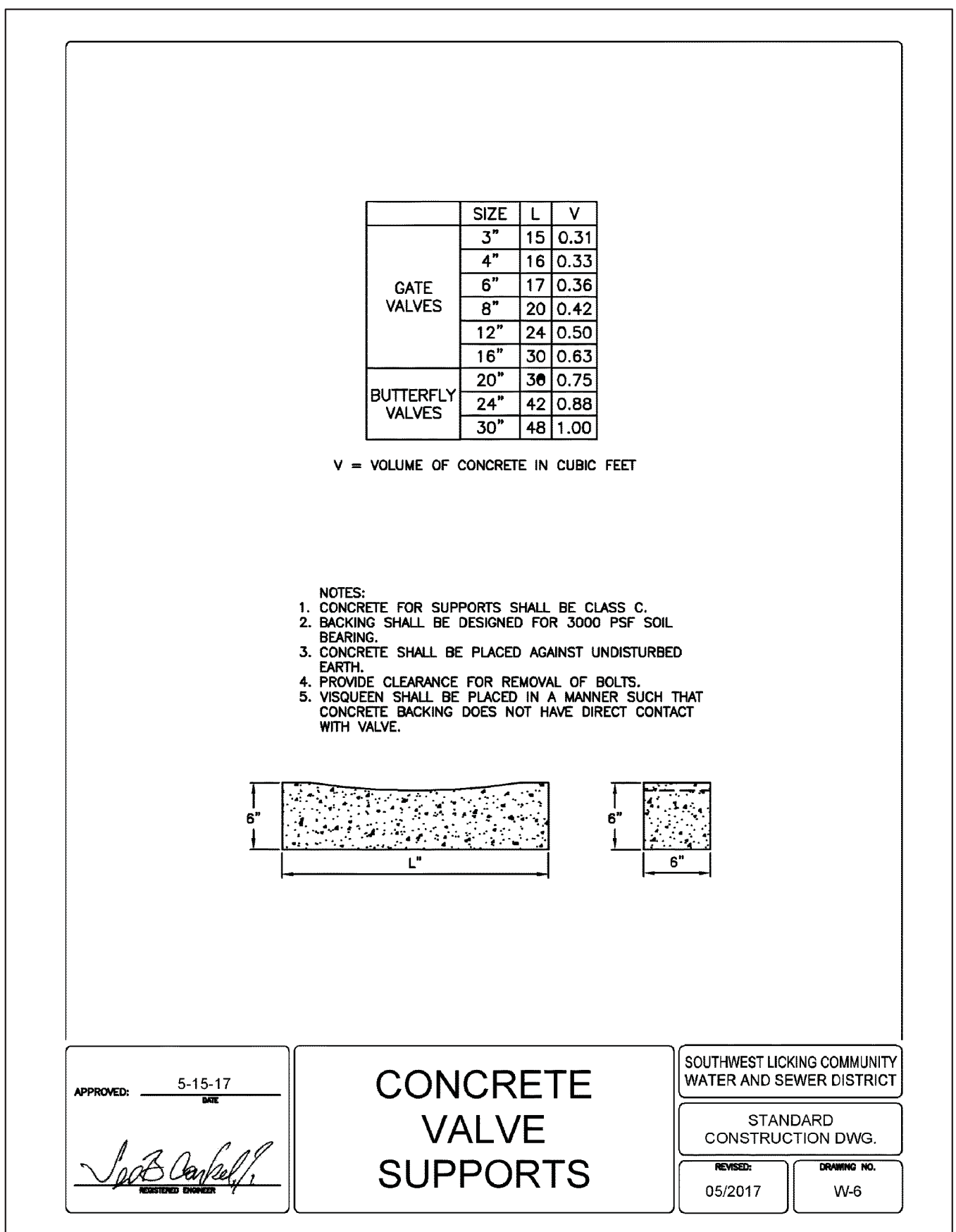
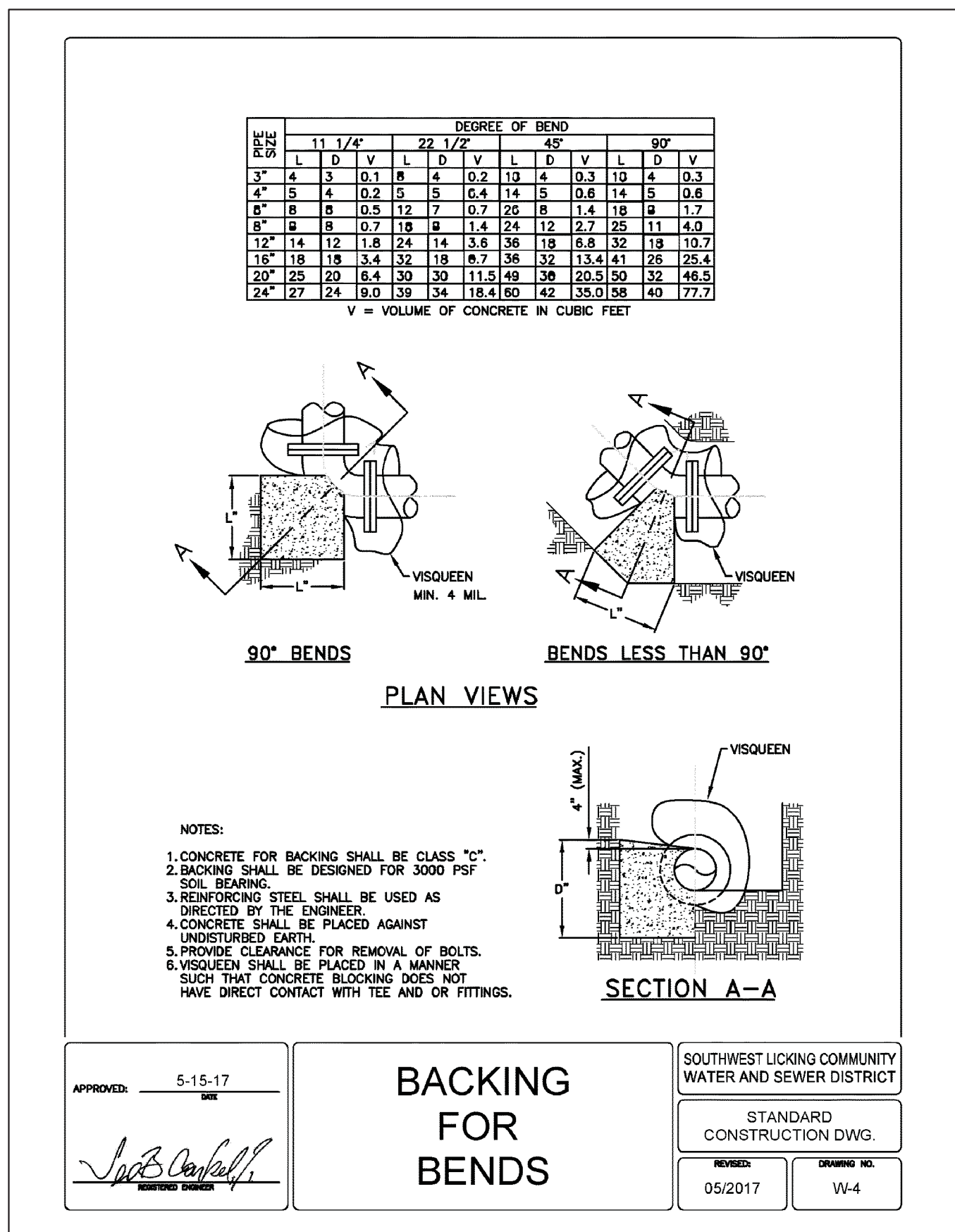
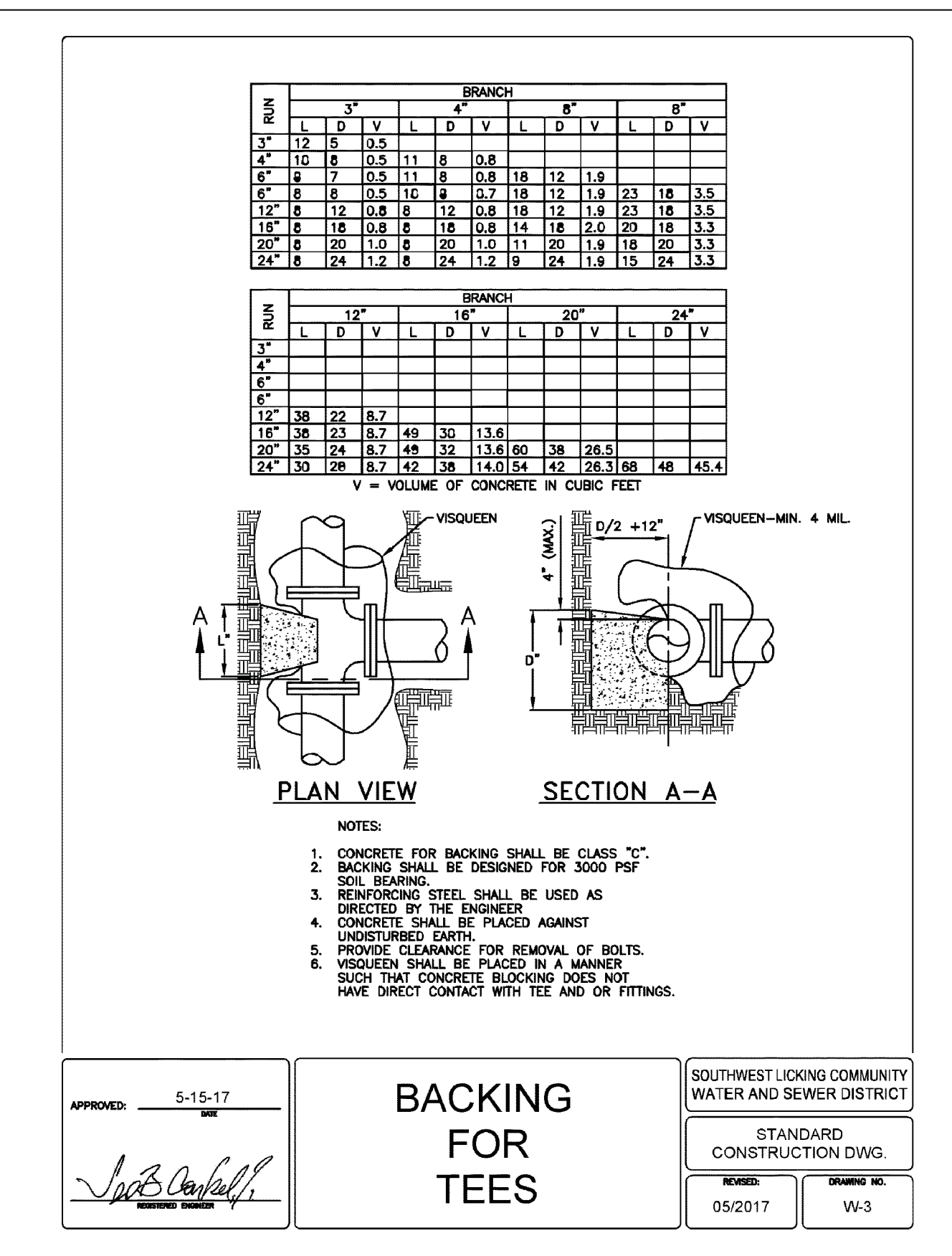
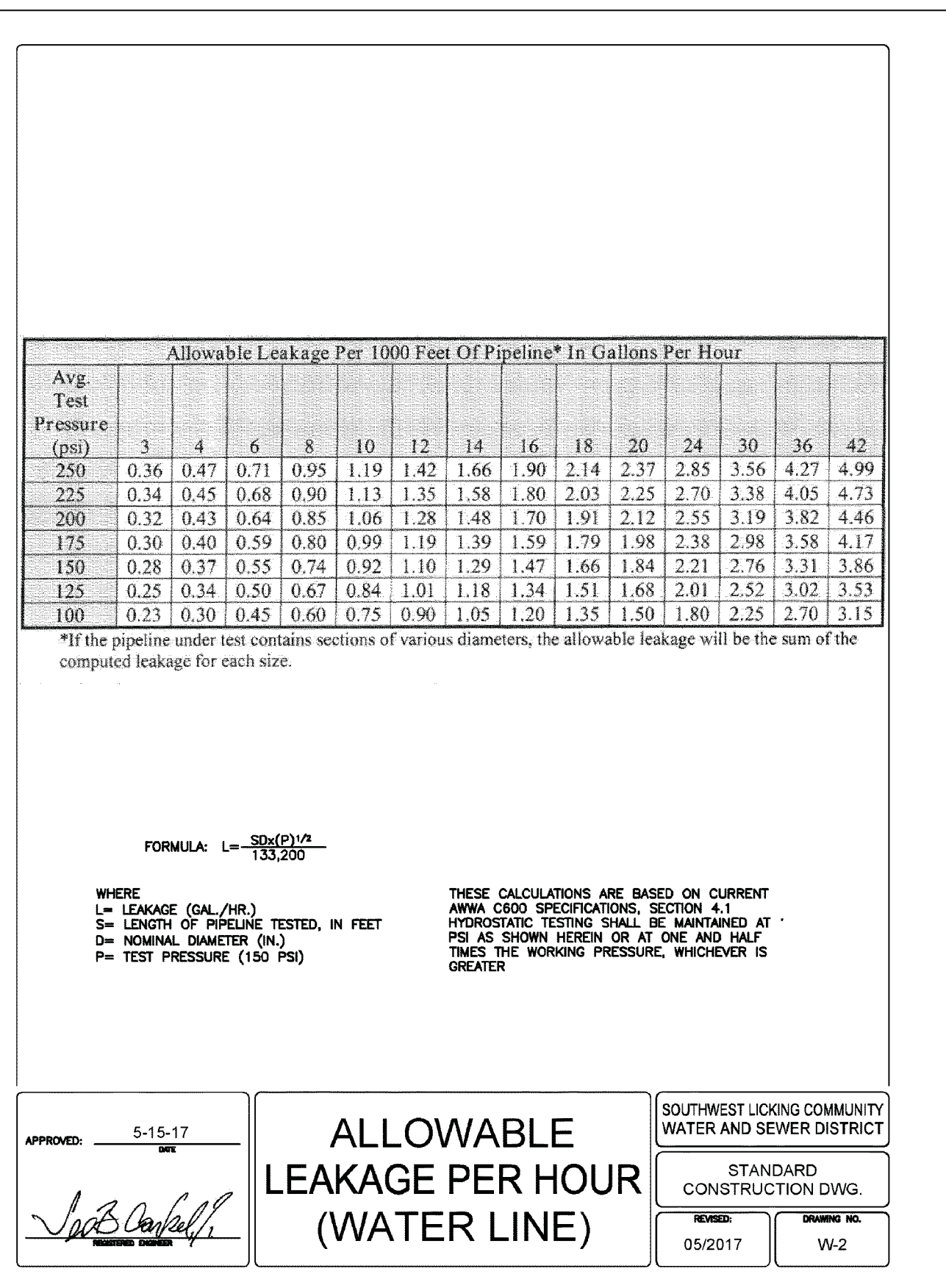
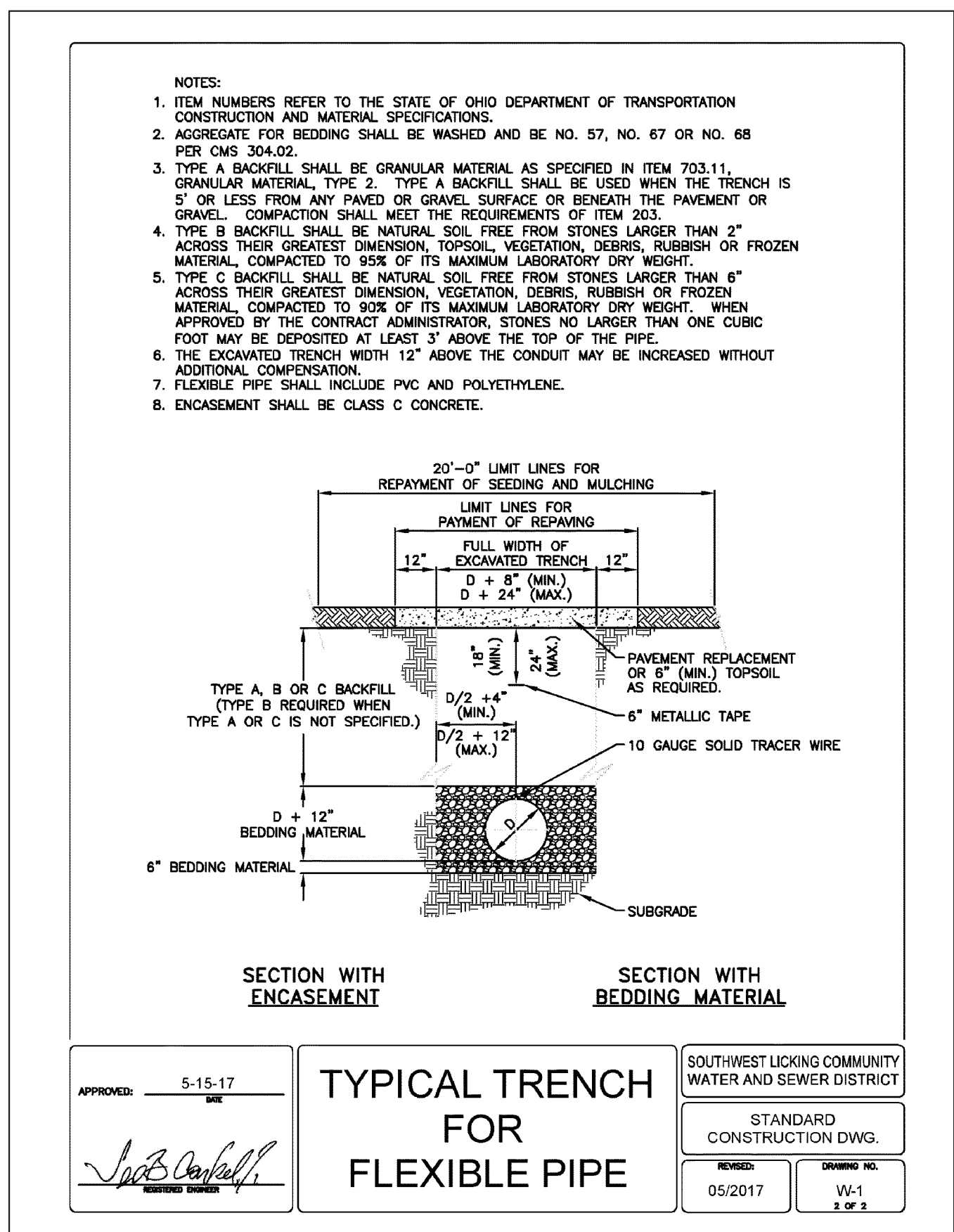
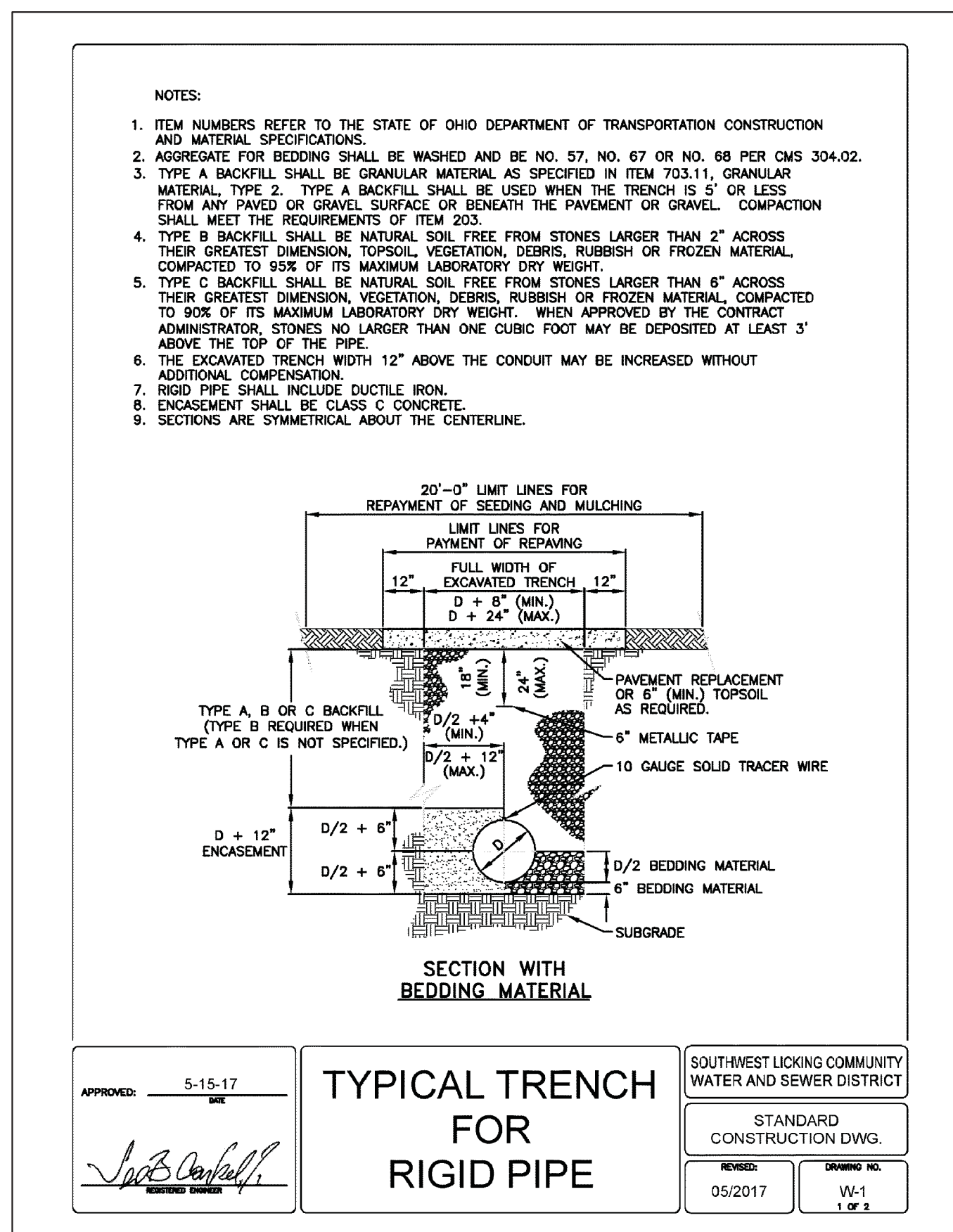


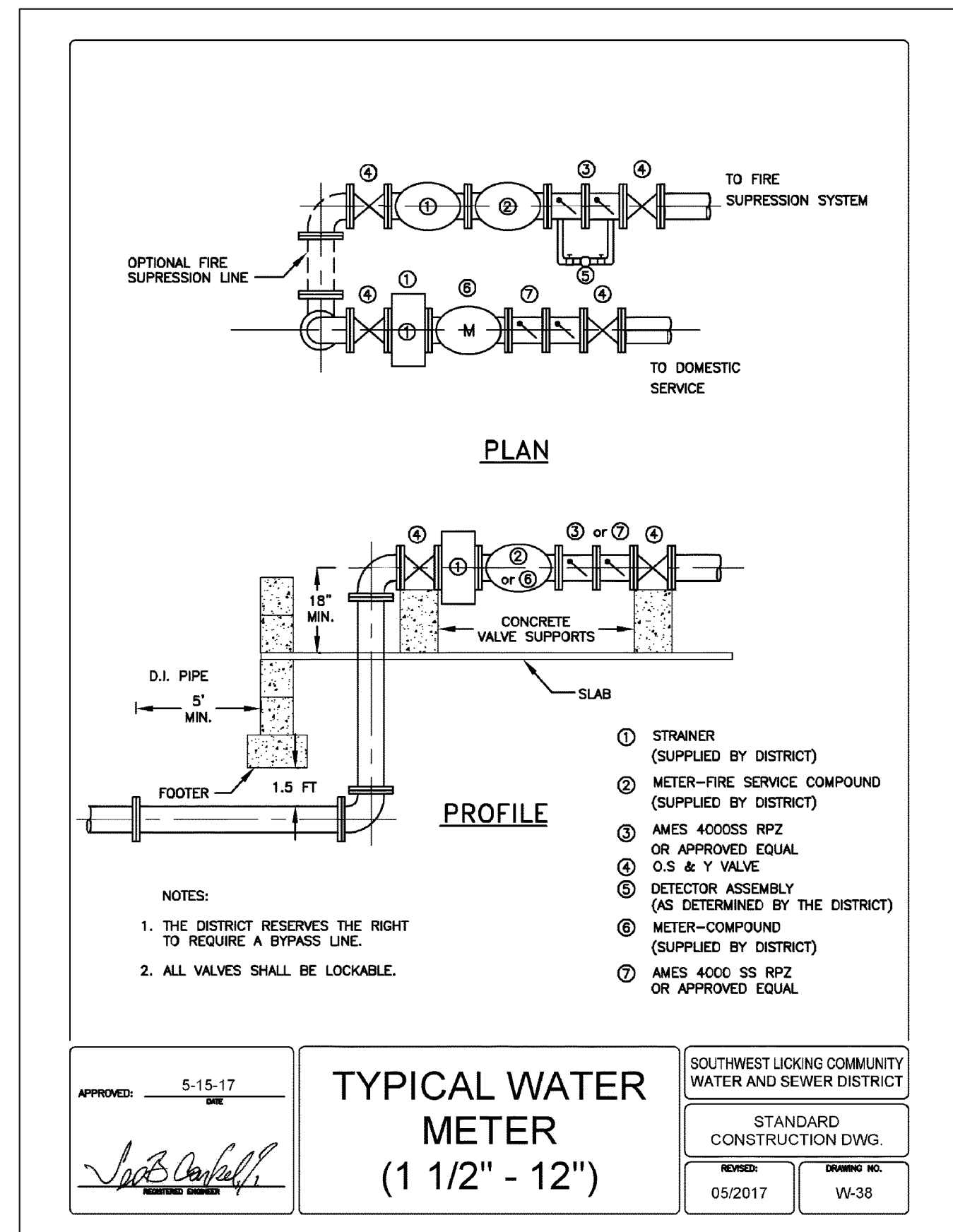
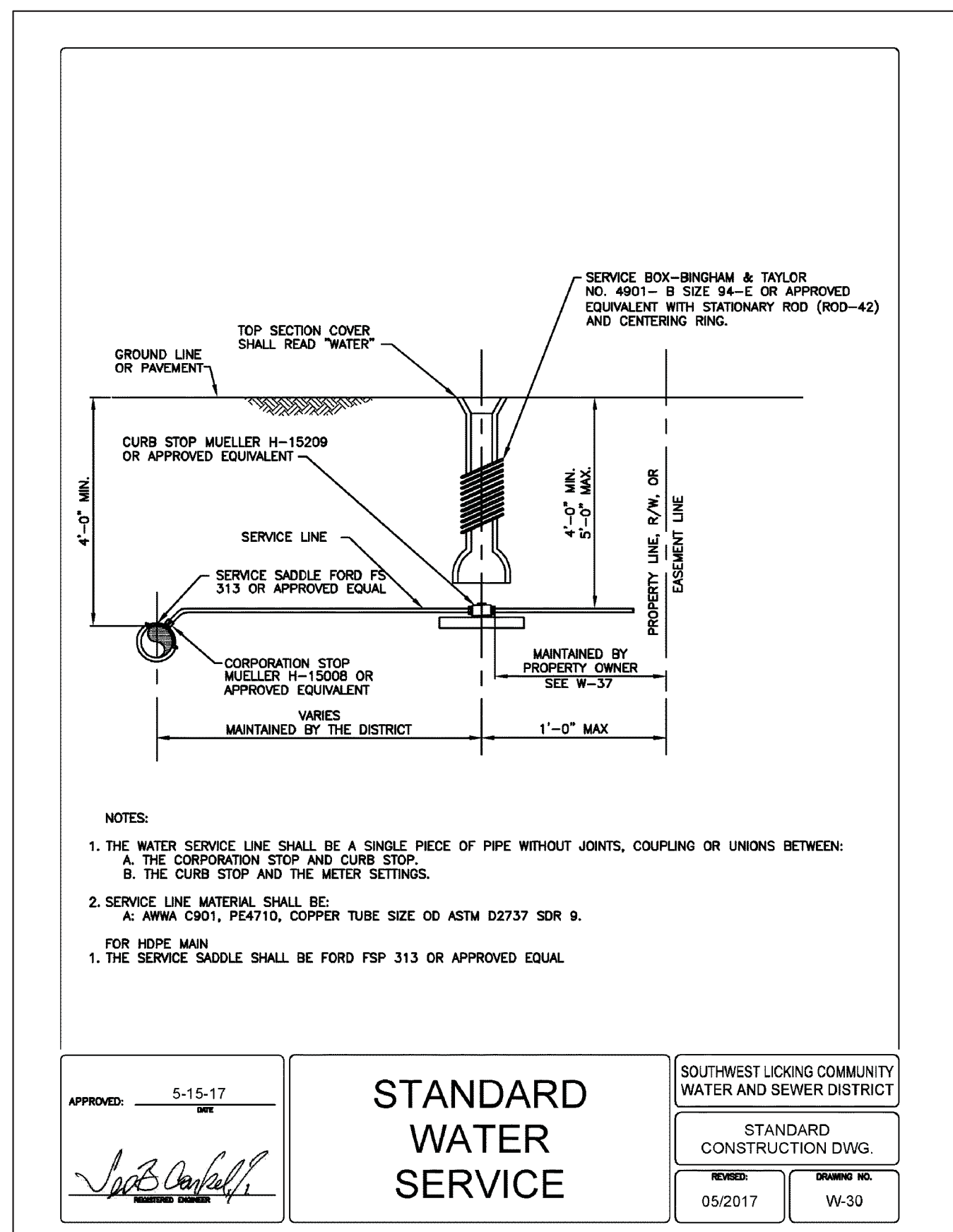
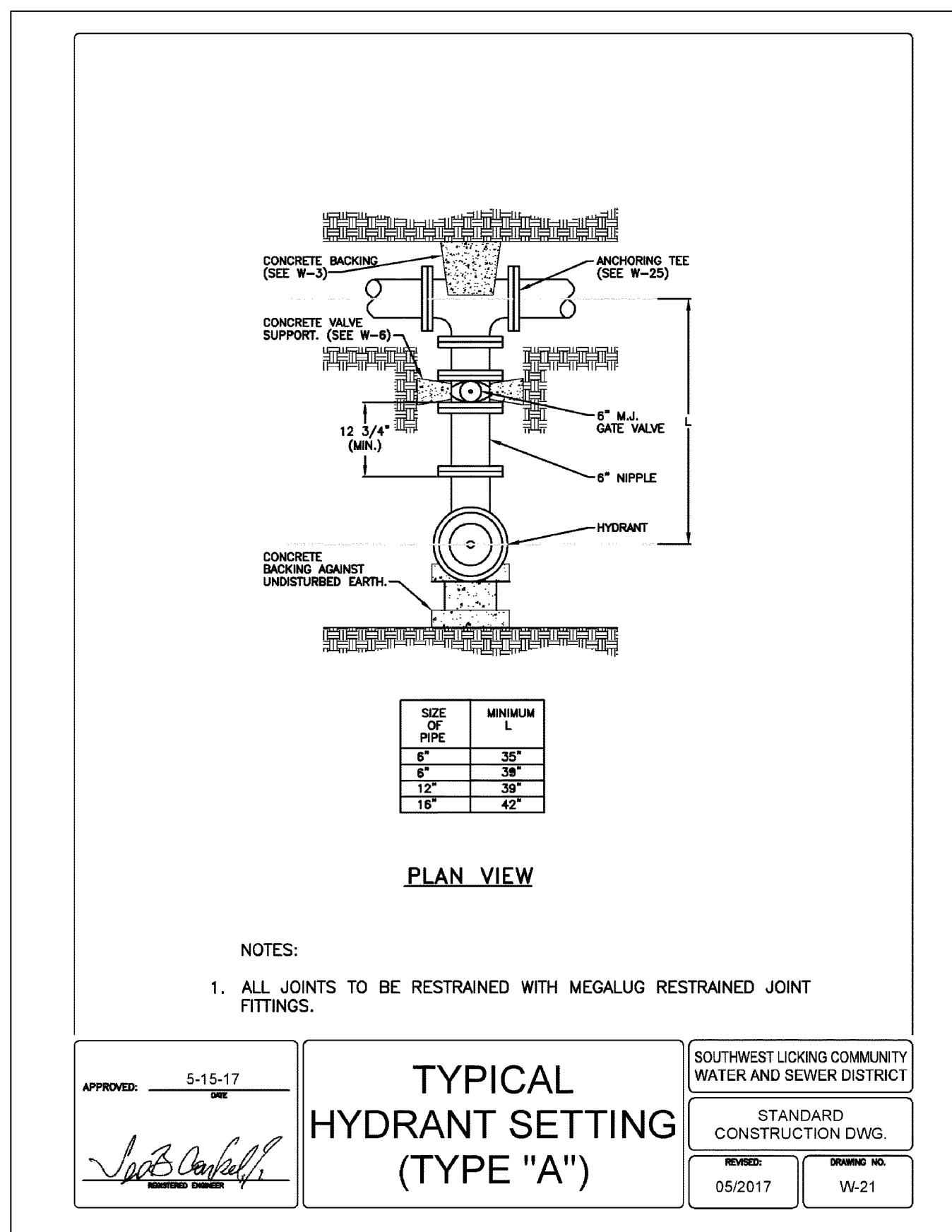
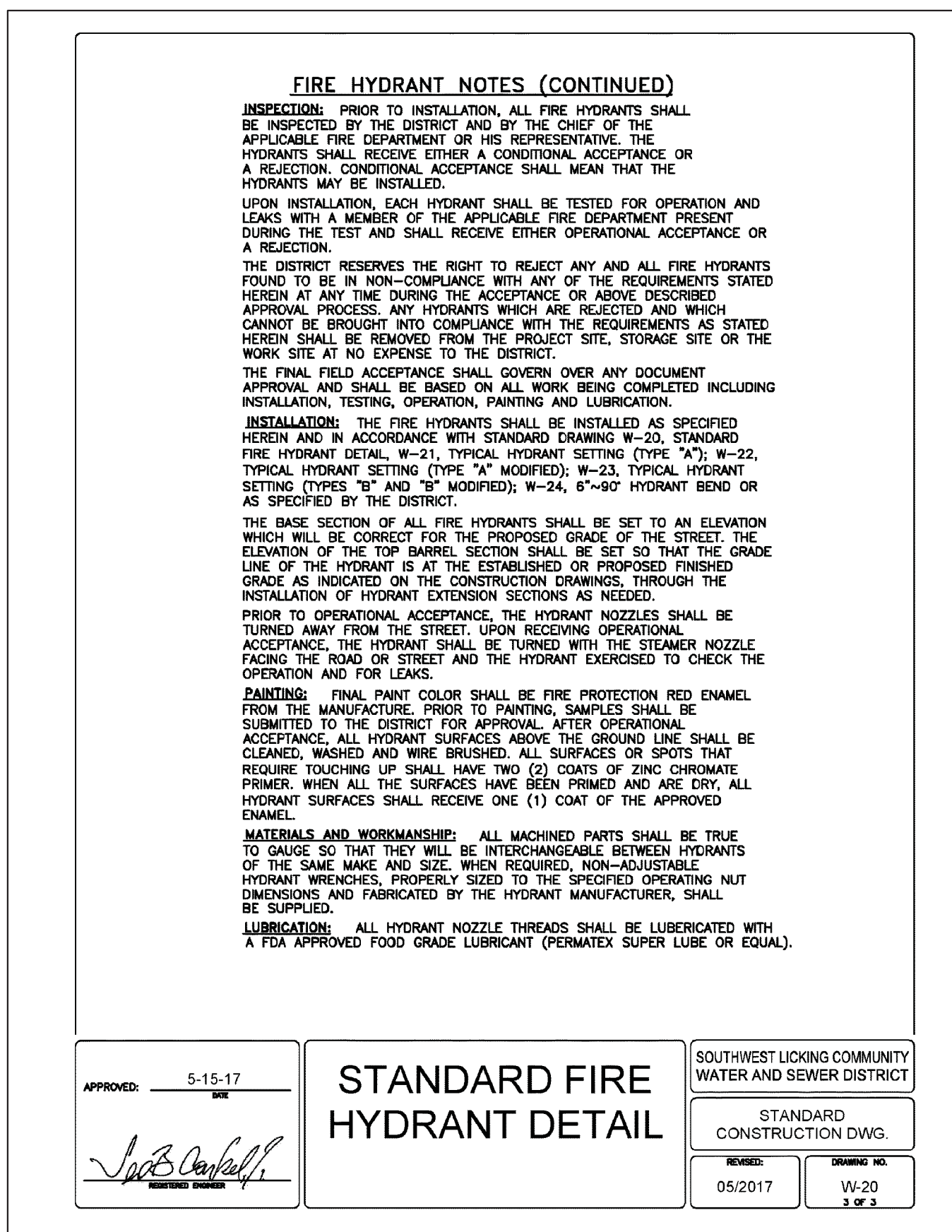
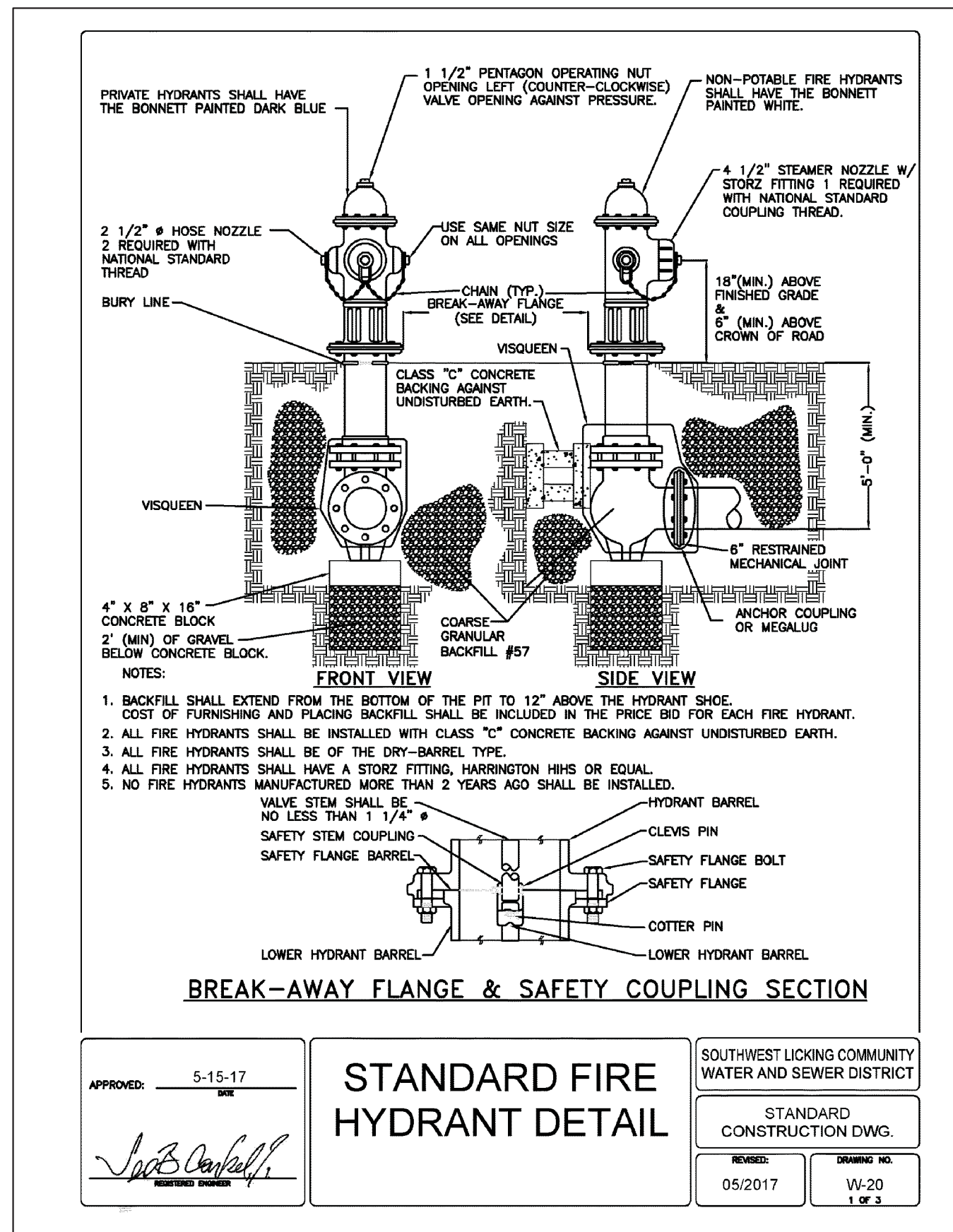
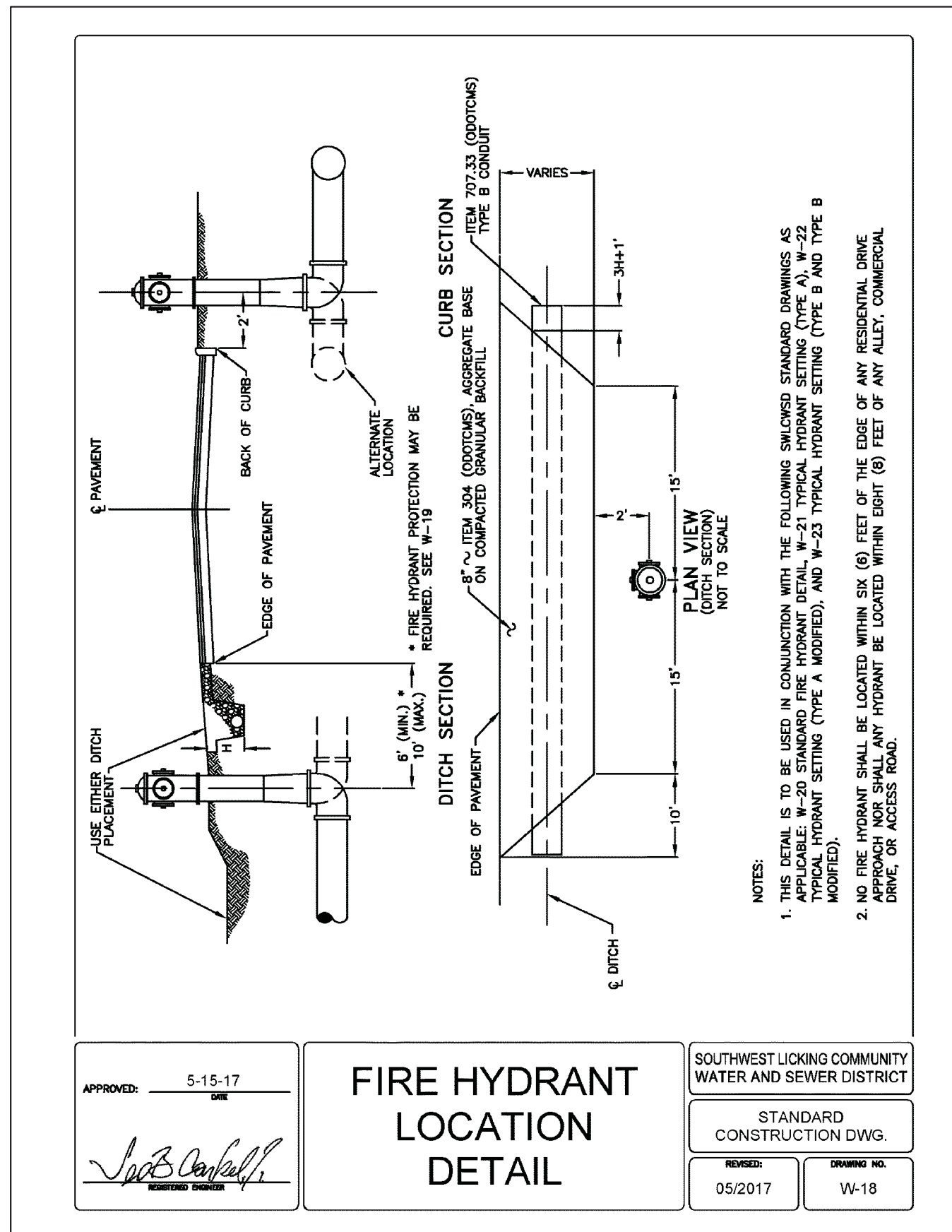
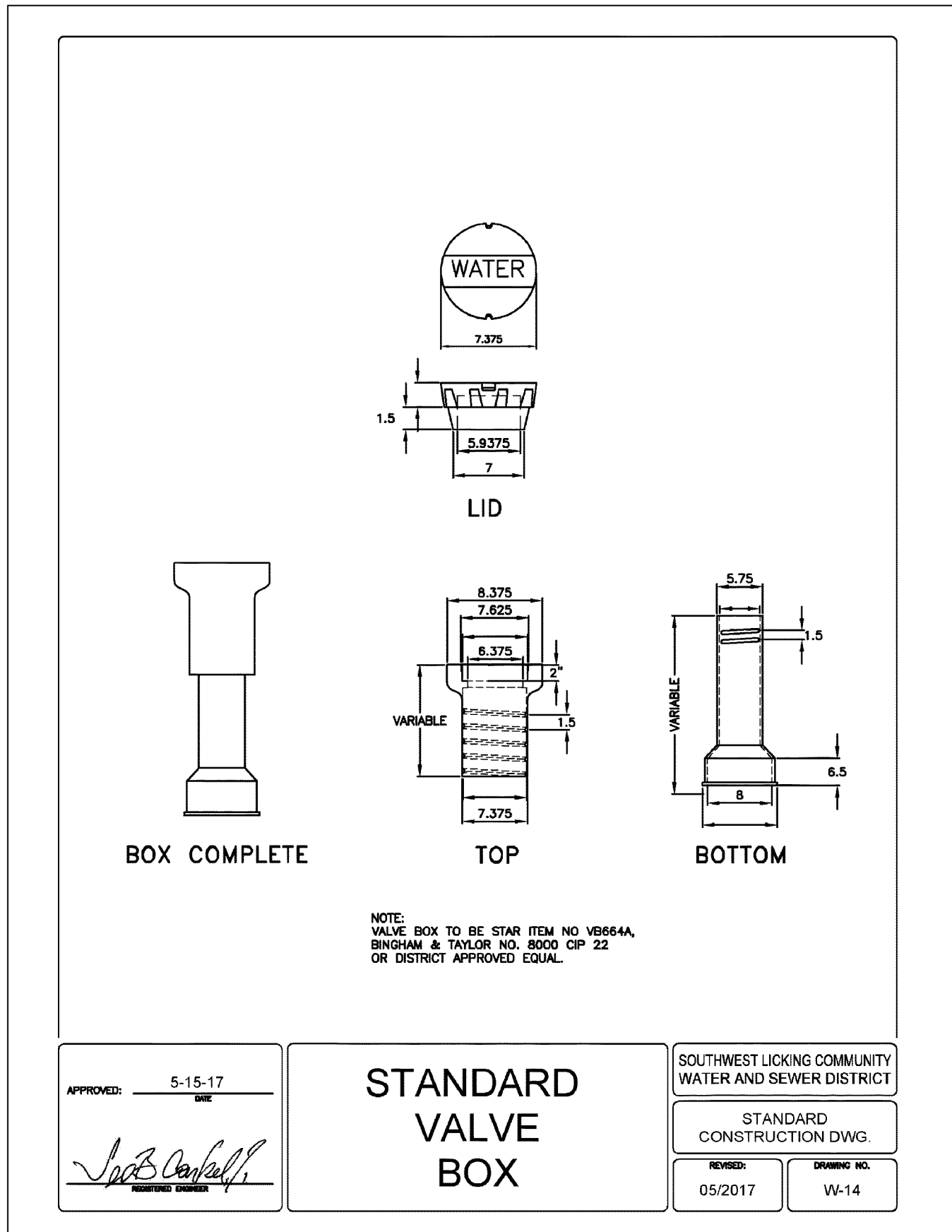
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WATERS  
ENGINEERING,

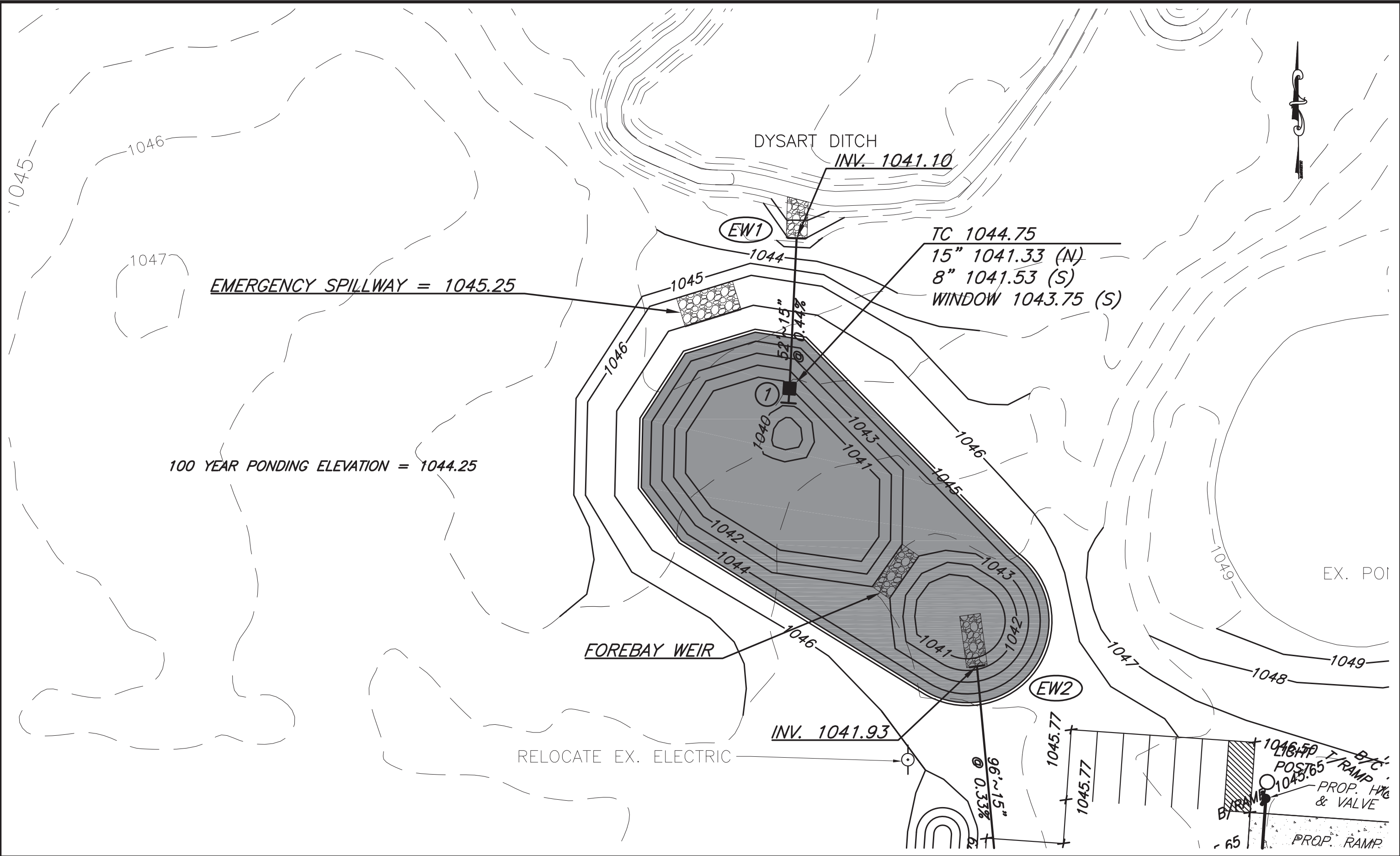
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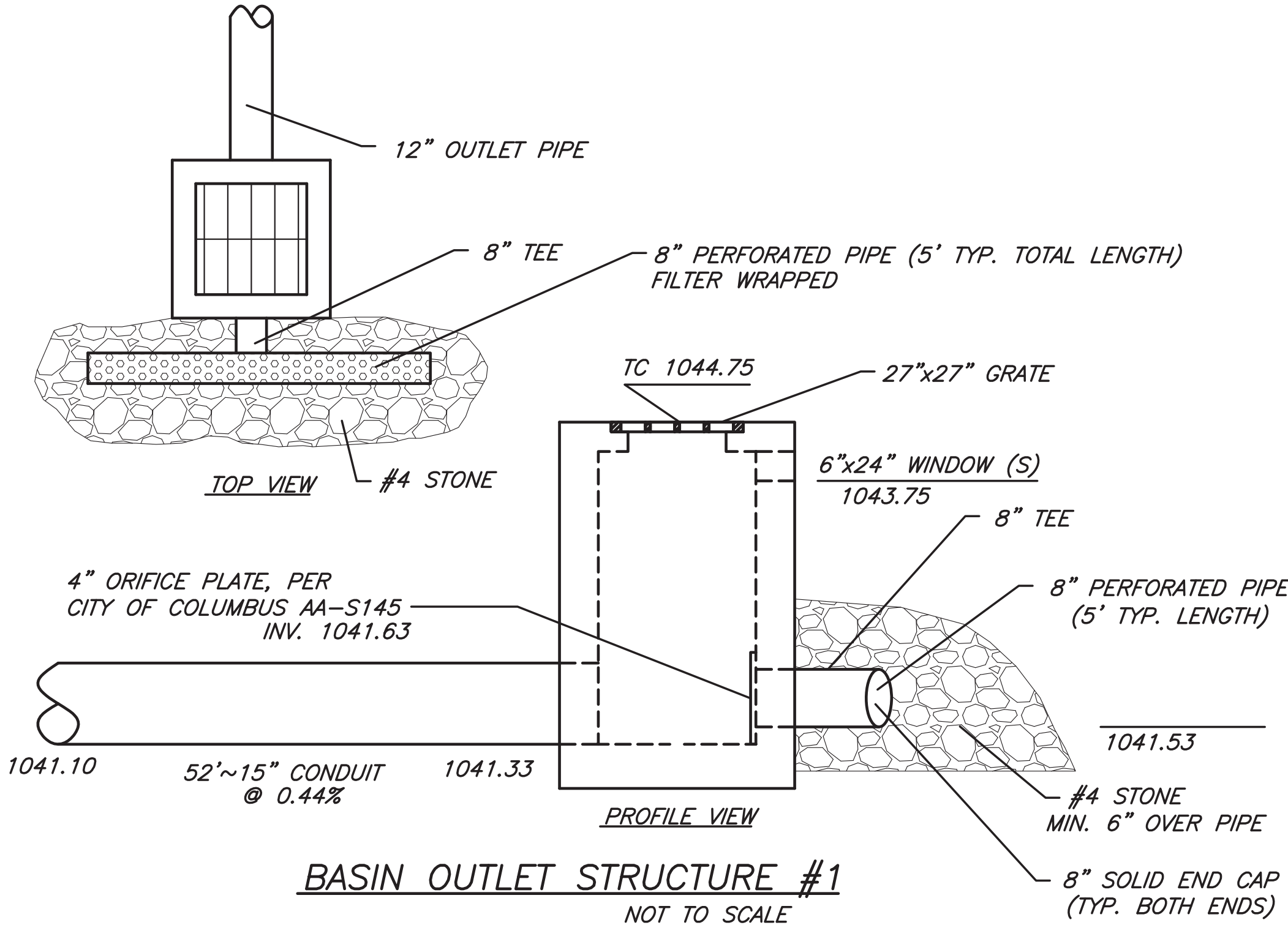
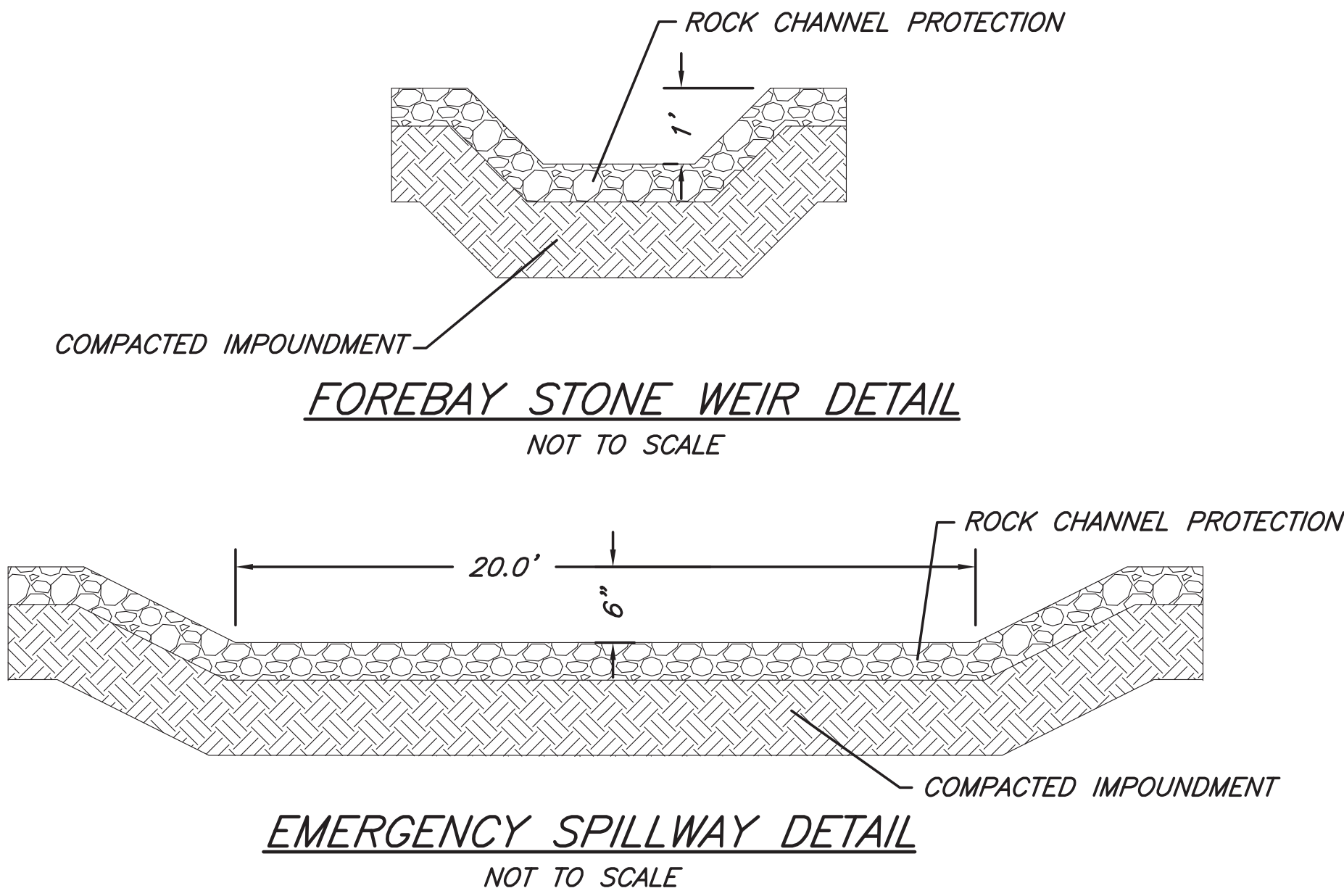
CROSSING WATERS ENGINEERING, INC. P.O. BOX 27, 260 S. MAIN ST., SUITE A SUGAR GROVE, OHIO 43155 (740) 746-0250	
TSTECH AMERICA CROSS DOCK IMPROVEMENT 6630 TAYLOR ROAD SW PATASKALA, LICKING COUNTY, OHIO STORM & WATER PROFILES	
CONTRACT	JOB NO.: CW-18016
DRAWN BY: RAB	SCALE: V = 5' ; H = 40'
CHECKED BY: JAS	DATE: 05/21/2019
APPROVED BY: RAB	SHEET: 10 OF 17







**BASIN PLAN**  
SCALE: 1" = 30'



**BASIN DATA**

**A. STORM EVENT RELEASE RATES**

STORM EVENT	ONSITE POST-DEVELOPED PEAK FLOW TO BASIN (CFS)	ALLOWABLE ONSITE RELEASE RATE (CFS)	BASIN DESIGN RELEASE RATE (CFS)	PONDING ELEVATION FOR GIVEN STORM (FEET)
1 YEAR	3.38	0.91	0.18	1041.98
2 YEAR	4.73	0.91	0.31	1042.34
5 YEAR	6.75	0.91	0.44	1042.88
10 YEAR	8.47	0.91	0.51	1043.29
25 YEAR	10.93	3.13	0.68	1043.80
50 YEAR	13.00	3.13	1.57	1044.03
100 YEAR	15.19	3.13	2.96	1044.25

**B. 100 YEAR STORM DETENTION**

CONTOUR ELEVATION (FEET)	SURFACE AREA (SQ. FEET)	BASIN STORAGE VOLUME (CU FT)	CUMULATIVE STORAGE VOLUME (CU FT)
1040.00	276	0	0
1041.00	2865	1343	1343
1042.00	3729	3287	4630
1043.00	6346	4979	9610
1044.00	8737	7509	17119
FOREBAY			
1041.00	676	0	0
1042.00	1102	880	880
1043.00	1647	1365	2246
MICRO POOL			
1039.00	87	0	0
1040.00	276	173	173
TOTAL BASIN STORAGE REQUIRED = 18,120.98 CU FT			
TOTAL BASIN STORAGE AVAILABLE = 19,538.00 CU FT			

NO PERSON SHALL DISCHARGE OR CAUSE TO BE DISCHARGED ANY STORM WATER, SURFACE WATER, GROUND WATER, ROOF RUNOFF, SUBSURFACE DRAINAGE, COOLING WATER OR UNPOLLUTED INDUSTRIAL PROCESS WATERS TO ANY SANITARY SEWER LOCATED WITHIN PATASKALA, OHIO, OR WITHIN ANY AREA UNDER THE JURISDICTION OF SAID CITY.

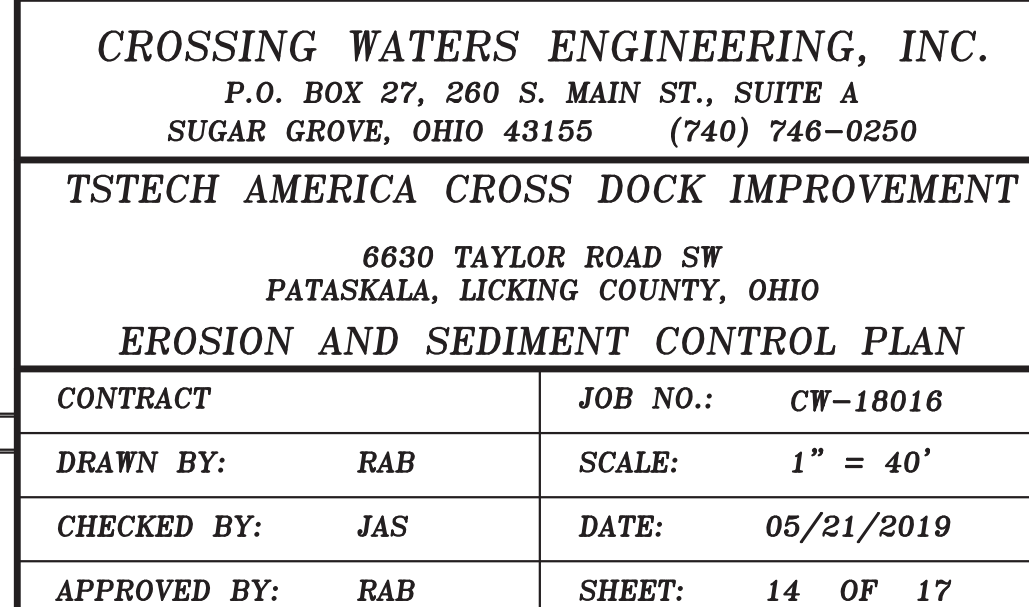
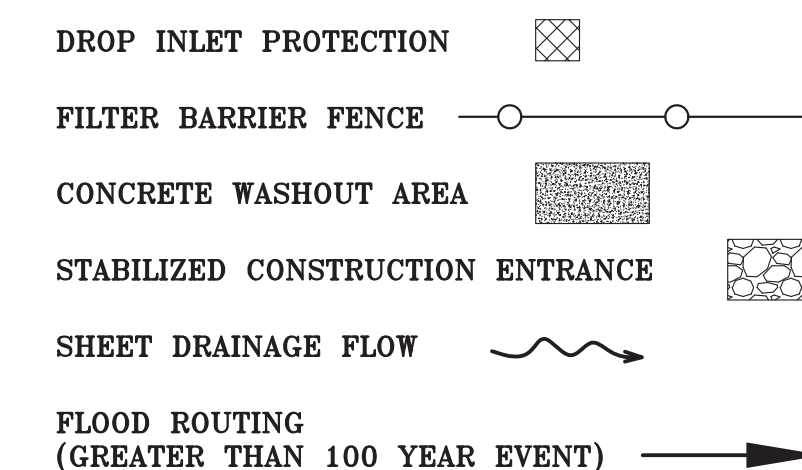
**CROSSING WATERS ENGINEERING,**

P.O. Box 27  
260 S. Main St., Suite A  
Sugar Grove, Ohio 43155  
(740) 746-0250  
www.crossingwaterseng.com

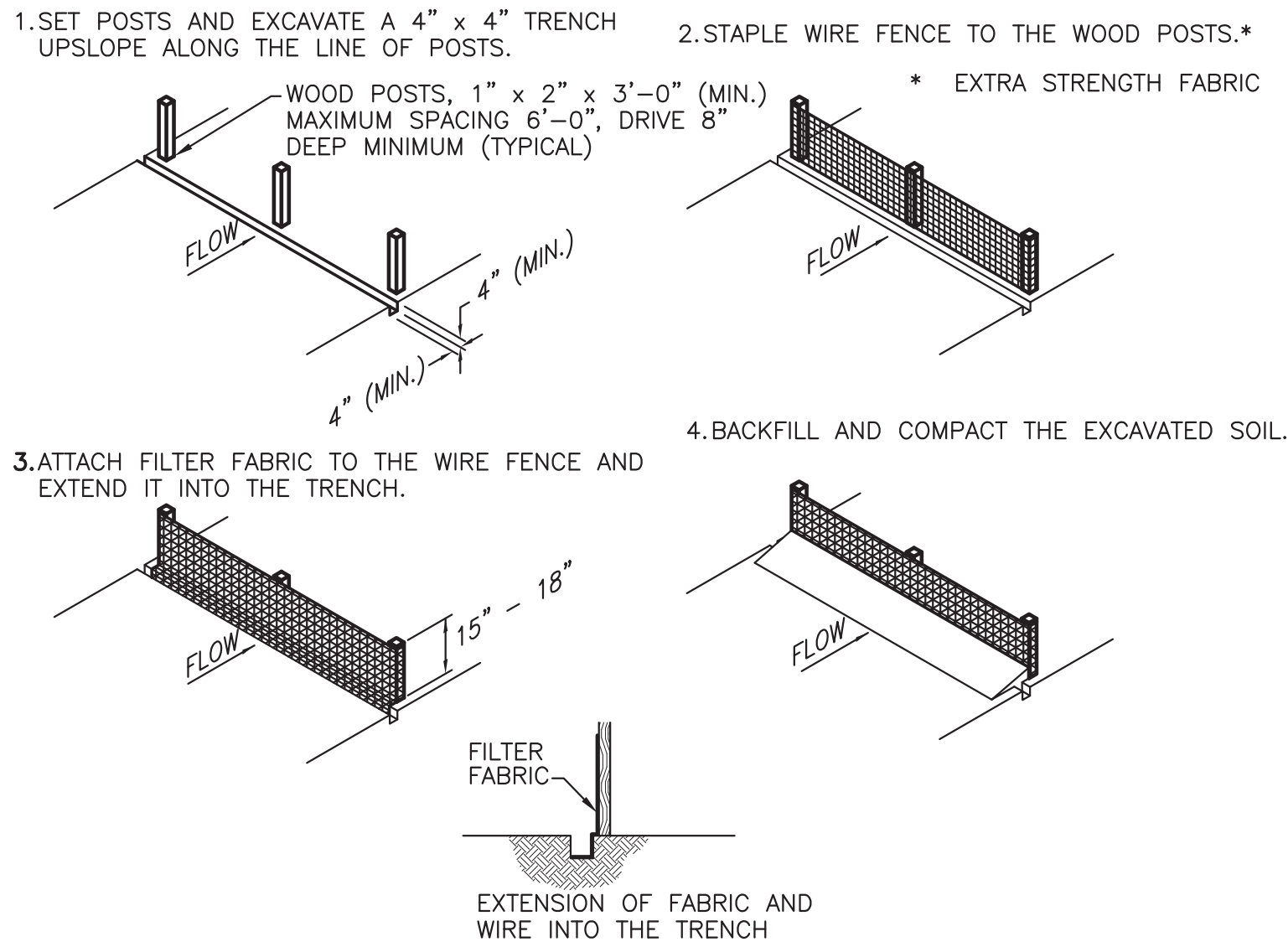


NO.	REVISION	BY	DATE	RECORD DRAWING

<b>CROSSING WATERS ENGINEERING, INC.</b> P.O. BOX 27, 260 S. MAIN ST., SUITE A SUGAR GROVE, OHIO 43155 (740) 746-0250	
<b>TSTECH AMERICA CROSS DOCK IMPROVEMENT</b> 6630 TAYLOR ROAD SW PATASKALA, LICKING COUNTY, OHIO DETAILS / DESIGN DRAWINGS	
CONTRACT	JOB NO.: CW-18016
DRAWN BY: RAB	SCALE: AS NOTED
CHECKED BY: JAS	DATE: 05/21/2019
APPROVED BY: RAB	SHEET: 13 OF 17



NO.	REVISION	BY	DATE	RECORD DRAWING
				BY:
				DATE:
				CONTRACTOR:
				CONSTRUCTED:
				NOTES:

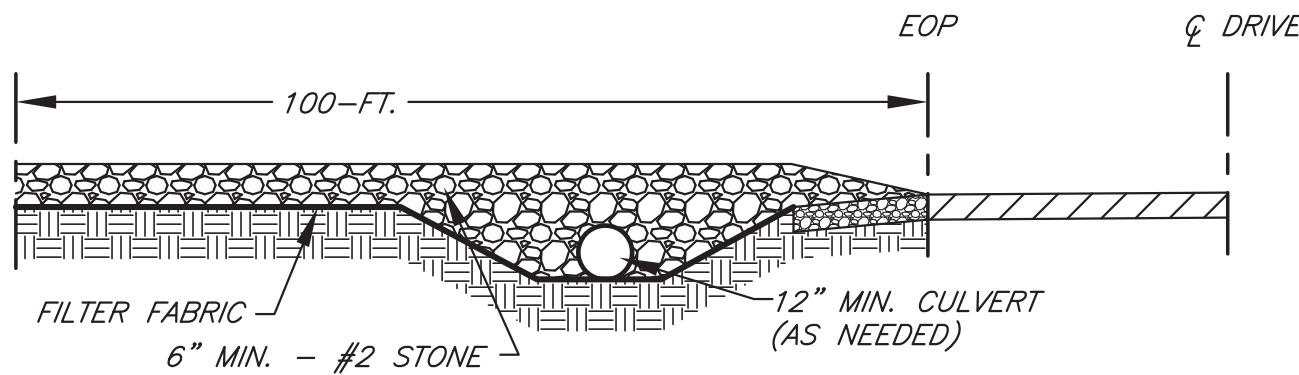


TYPICAL FILTER BARRIER CONSTRUCTION DETAIL

TEMPORARY CONSTRUCTION ENTRANCE

NOT TO SCALE

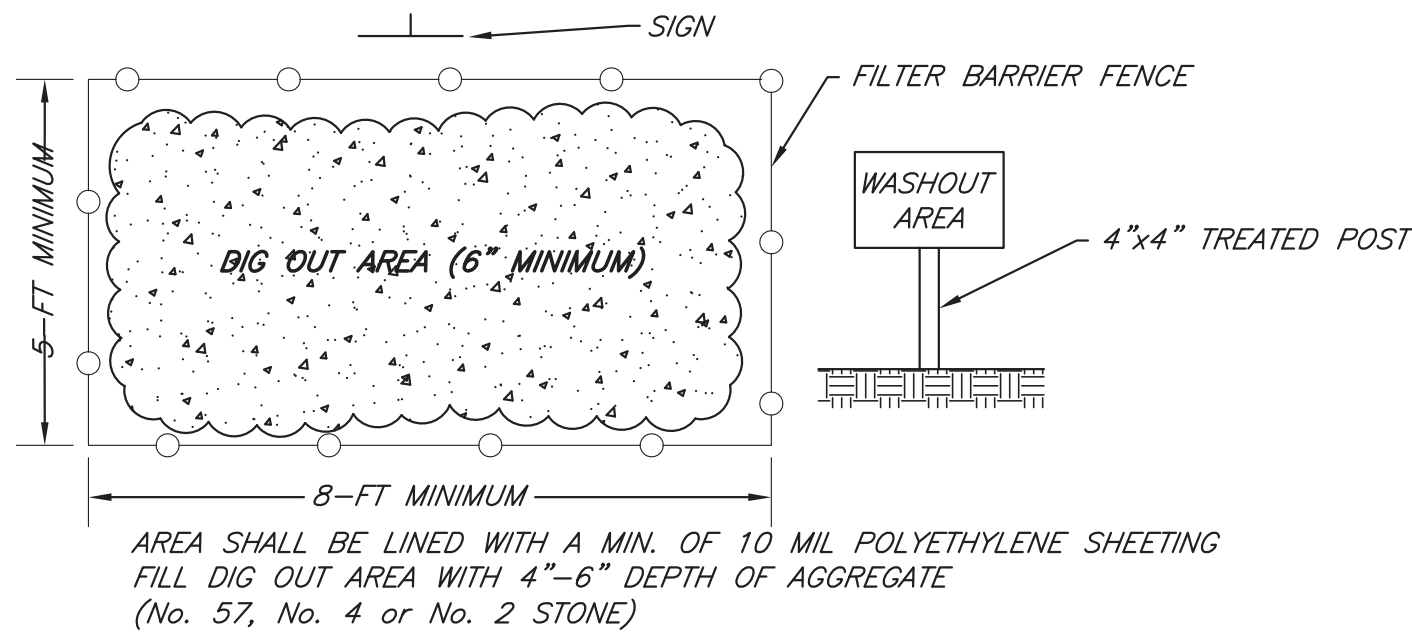
THE TEMPORARY CONSTRUCTION ENTRANCE SHALL COMPLY WITH THE CITY OF COLUMBUS, STANDARD DRAWING 2222, AND THE FOLLOWING DETAIL:



NOTE:

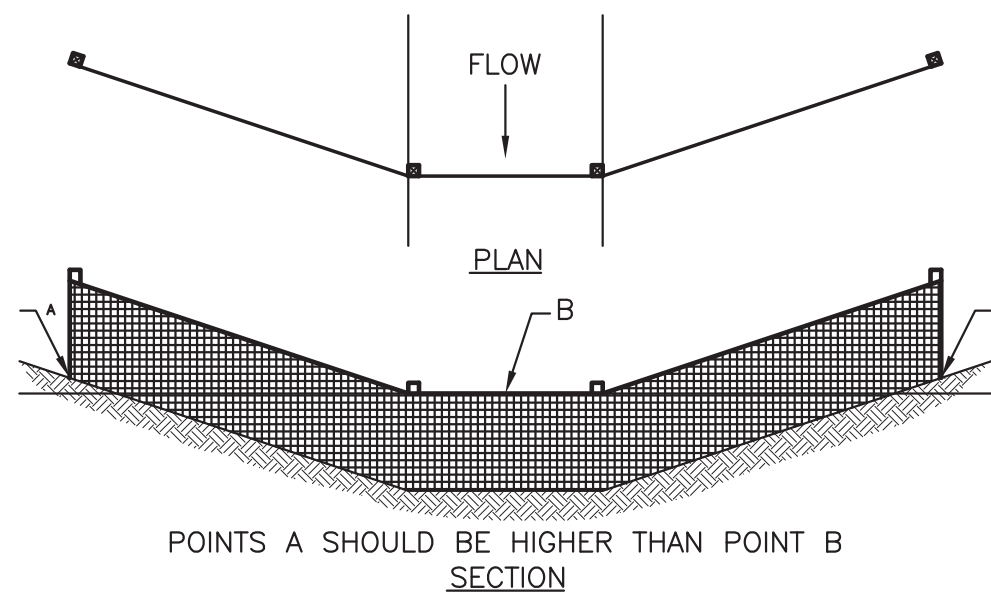
MINIMUM 20'-FT. WIDTH FOR DRIVING SURFACE, AND MINIMUM 22'-FT. WIDTH OF BASE SPREAD TO ACCOMMODATE THE 30° ANGLE OF REPOSE FOR #2 STONE.

AREA IN WHICH THE TEMPORARY CONSTRUCTION ENTRANCE IS TO BE INSTALLED, SHALL ALSO BE "ROCK-HOUSED" FOLLOWING ITS REMOVAL TO ENSURE NO STONES LARGER THAN 1-INCH ARE LEFT BEHIND BEFORE ITEM #659 IS APPLIED.



CONCRETE WASHOUT AREA DETAIL

NOT TO SCALE



PROPER PLACEMENT OF A  
FILTER BARRIER IN A DRAINAGEWAY  
NOT TO SCALE

MAINTENANCE

SILT FENCES AND FILTER BARRIERS SHALL BE INSPECTED IMMEDIATELY AFTER EACH RAINFALL AND AT LEAST DAILY DURING PROLONGED RAINFALL. ANY REQUIRED REPAIRS SHALL BE MADE IMMEDIATELY. SHOULD THE FABRIC ON A SILT FENCE OR FILTER BARRIER DECOMPOSE OR BECOME INEFFECTIVE PRIOR TO THE EXPECTED USABLE LIFE AND THE BARRIER IS STILL NECESSARY, THE FABRIC SHALL BE REPLACED PROMPTLY. SEDIMENT DEPOSITS SHOULD BE REMOVED AFTER EACH STORM EVENT. THEY MUST BE REMOVED WHEN DEPOSITS REACH APPROXIMATELY ONE-HALF THE HEIGHT OF THE BARRIER. ANY SEDIMENT DEPOSITS REMAINING IN PLACE AFTER THE SILT FENCE OR FILTER BARRIER IS NO LONGER REQUIRED SHALL BE DRESSED TO CONFORM WITH THE EXISTING GRADE, PREPARED, AND SEEDED.

NOTE:

STREET CLEANING (ON AN AS-NEEDED BASIS) IS REQUIRED THROUGH THE DURATION OF THIS CONSTRUCTION PROJECT. THIS INCLUDES SWEEPING, POWER CLEANING AND (IF NECESSARY) MANUAL REMOVAL OF DIRT OR MUD IN THE STREET GUTTERS.

EROSION AND SEDIMENTATION CONTROL NOTES

SILTATION AND EROSION SHALL BE CONTROLLED BY MINIMIZING THE TIME THE SITE IS DENUDEED BY TEMPORARY SEEDING, PERMANENT SEEDING, AND THE USE OF VARIOUS MECHANICAL MEASURES. THESE DEVICES ARE TO BE MAINTAINED THROUGHOUT THE CONSTRUCTION PHASE OF THIS PROJECT. ONCE THE SITE AREA HAS BEEN STABILIZED, THE MECHANICAL MEASURES SHALL BE REMOVED WITH THE APPROVAL OF THE ENGINEER.

ALL MEASURES SHALL BE MAINTAINED UNTIL ALL CONSTRUCTION UNDER THE CONTRACT HAS BEEN COMPLETED AND ACCEPTED BY THE OWNER. MAINTENANCE SHALL INCLUDE ALL REPAIRS TO THOSE AREAS AND MEASURES DAMAGED BY EROSION, CONSTRUCTION EQUIPMENT, WEATHER, FIRE, AND/OR OTHER CAUSES INCLUDING "ACTS OF GOD."

THE TEMPORARY CONSTRUCTION ENTRANCES SHALL BE MAINTAINED IN A STATE THAT SHALL PREVENT TRACKING OR FLOW OF MUD ONTO THE PUBLIC RIGHT-OF-WAY. THIS WILL BE ACCOMPLISHED BY USE OF A GRAVEL CONSTRUCTION ENTRANCE, AND THE REPAIR AND/OR CLEANOUT OF ANY DEVICES USED TO TRAP THE SEDIMENT. ANY MATERIALS SPILLED, DROPPED, OR TRACKED FROM THE CONSTRUCTION SITE ONTO THE ROADWAYS OR INTO THE STORM SYSTEM MUST BE REMOVED IMMEDIATELY.

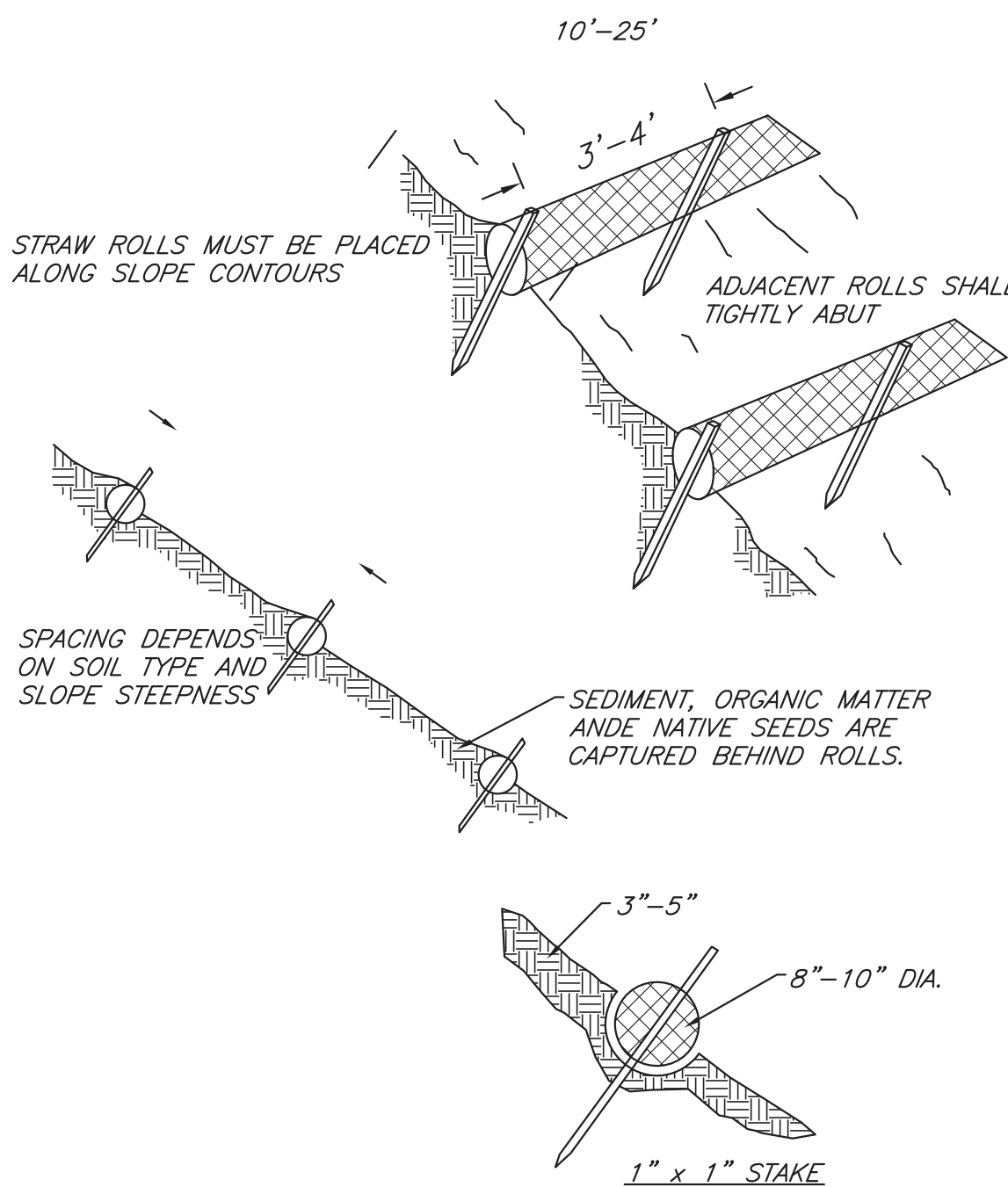
SEDIMENT DEPOSITS IN SEDIMENT TRAPS AND BASINS SHALL BE REMOVED AND THE MEASURES RESTORED TO THEIR ORIGINAL DESIGN PRIOR TO THE SEDIMENT OBTAINING THE ONE-HALF DESIGN HEIGHT OF THE DEVICE. ALL SEDIMENT MATERIAL SHALL BE DISPOSED OF BY AN ACCEPTABLE MEANS. ANY SEDIMENT DEPOSITS REMAINING IN PLACE AFTER THE MEASURES ARE NO LONGER REQUIRED SHALL BE PREPARED TO CONFORM TO THE FINAL GRADING PLANS, SEEDING, AND MULCHED.

THE CONTRACTOR SHALL PLACE STRUCTURES FOR EROSION CONTROL AT CATCH BASINS IMMEDIATELY AFTER THEIR CONSTRUCTION. THE CONTRACTOR SHALL REMOVE STRAW BALES WHEN GRASS HAS BEEN ESTABLISHED AS 2" HIGH OVER DRAINAGE AREA FLOWING TO CATCH BASINS. THE ENGINEER SHALL DIRECT EROSION CONTROL AT CATCH BASIN IF REQUIRED.

TOPSOIL SHALL BE REMOVED FROM PROPOSED RIGHT-OF-WAY, MOUNDING, AND EMBANKMENT AREAS PRIOR TO CONSTRUCTION OF PAVEMENT, MOUNDING, AND EMBANKMENT AREA. TOPSOIL SHALL BE STOCKPILED FOR RESPREADING AS DIRECTED BY OWNER.

THE CONTRACTOR SHALL NOT DISTURB AREAS OUTSIDE CONSTRUCTION LIMITS UNLESS AUTHORIZED BY THE PROPERTY OWNER.

THE CONTRACTOR SHALL DISPOSE OF STREET, STOCKPILE, AND OVERLOT EXCAVATION AS DIRECTED BY THE OWNER.



NOTE:

STRAW ROLL INSTALLATION REQUIRES THE PLACEMENT AND SECURE STAKING OF THE ROLL IN A TRENCH, 3'-5" DEEP, DUG ON CONTOUR, RUNOFF MUST NOT BE ALLOWED TO RUN UNDER OR AROUND ROLL.

STRAW ROLL DETAIL

NOT TO SCALE

NOTE:

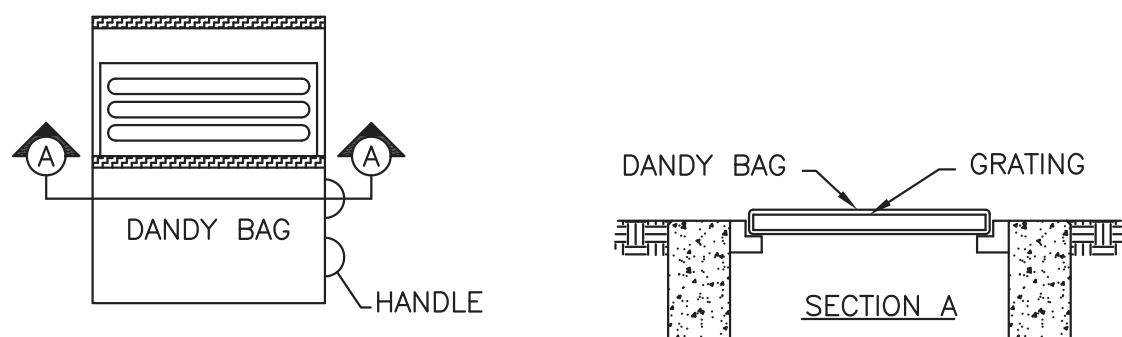
THE USE OF STRAW WATTLES HAS PROVEN TO BE A VERSATILE AND EFFECTIVE ESC BMP, ESPECIALLY IN RESIDENTIAL SETTINGS. STRAW WATTLES MAY SUBSTITUTED FOR SILT FENCE IN LINEAR INSTALLATIONS.

ADDITIONALLY:

THE USE OF COMPOST FILTER SOCKS AND COMPOST BLANKETS ARE GAINING WIDER ACCEPTANCE NATIONWIDE. THEY ARE NOW APPROVED FOR USE ON ALL COLUMBUS SWP3 PLANS & CONSTRUCTION SITES.

NOTE:

DIRECT DISCHARGE OF SEDIMENT LADEN WATER TO THE CITY'S SEWER SYSTEM OR A RECEIVING STREAM IS A VIOLATION OF OHIO EPA AND CITY OF PATASKALA REGULATIONS. THE CONTRACTOR WILL BE HELD LIABLE FOR THE VIOLATION AND SUBSEQUENT FINES.



THIS METHOD TO BE USED AT ALL DROP INLETS. USE EITHER GRATE SACK BY JMD PRODUCTS, DANDY BAG BY DANDY PRODUCTS OR APPROVED EQUAL. CONTRACTOR TO CLEAN FILTERS AFTER STORM EVENTS AND MAINTAIN UNTIL COMPLETION OF PAVING IN PAVED AREAS OR ESTABLISHMENT OF GRASS IN SEEDED AREAS. TO BE USED ON STRUCTURES 2 - 4

FILTER FABRIC INLET PROTECTION

EROSION AND SEDIMENT CONTROL NARRATIVE:

Plan Engineer:

CROSSING WATERS ENGINEERING, INC.  
260 South Main Street, Suite A  
P.O. Box 27  
Sugar Grove, Ohio 43155  
Phone/Fax: (740) 746-0250

Owner:

TsTech AMERICAS, INC.  
8458 East Broad Street  
Reynoldsburg, Ohio 43068  
Phone: (614) 374-1761  
Email: charlie\_kelly@tstna.com

Existing Site Description:

The scope of the Fully Developed Site Grading Area encompasses approximately 3.60 Acres on a vacant lot. Current site drainage is considered poor, with an ultimate drainage point being located in Dysart Ditch, which runs through the property from East to West.

Existing Site Drainage:

The parcel is bordered to the North by a parcels owned by COLUMBUS & OHIO RIVER RAIL ROAD CO. WAGGONER RD. (171-000034-00) & R&S TAYLOR ROAD, LLC. (063-140802-00.000), to the West by FRANKLIN COUNTY and a parcel owned by TSTECH USA CORP (171-000001-00), to the South by a parcel owned by TAYLOR ROAD INVESTMENTS, LLC. (063-142002-01.000), and to the East by TAYLOR ROAD.

Site Drains To:

The site naturally drains from East to West, through the original un-developed site. This development shall ultimately drain to a watercourse known as the Dysart Ditch. Ultimately, the site would be considered part of the Blacklick Creek Watershed.

Critical Areas:

Careful consideration should take place to the currently developed properties and ditches surrounding this site, public roadways shall also be emphasized.

Erosion and Sediment

Control Measures:

Erosion and sediment runoff shall be maintained and controlled by both sediment control fencing along the perimeter of the site and filter fabric drop inlet protection at all storm structures with grated lids.

Maintenance:

Maintenance of the Erosion and Sediment Control Items shall be in accordance with the City of Pataskala, Licking County and Ohio EPA Current Regulations.

Construction sequence:

(Erosion & Sediment)

1. Install Construction Entrance and Concrete Washout Area.
2. The Contractor shall install the required amount of Sediment Fence and inlet protection prior to any construction activity in accordance with the plan notes and details.
3. The Contractor shall perform Site Earthwork Activity in accordance with the plan notes and details.
4. The Contractor shall install Storm Sewer and Protect Inlets in accordance with the plan notes and details.
5. The Contractor shall place Seeding and Mulching as necessary to re-establish all denuded areas.
6. The Contractor shall properly maintain all Erosion and Sediment Control Devices, throughout all of the construction activities.
7. The Contractor shall remove and dispose of all Sediment and Erosion Control Devices only after areas have established vegetative cover and approval of the City of Columbus.

Note: The on-site contact responsible for Erosion & Sediment Control is:

Name	Phone Number
BMP Installation	Stimmel Conscruction (740) 380-9800
BMP Maintenance	Stimmel Construction (740) 380-9800
Site Stabilization	Stimmel Construction (740) 380-9800
BMP Removal	Stimmel Construction (740) 380-9800

Site Representative: Jeff Stimmel

Owner Representative: Charles Kelly, Plant Engineer



CROSSING WATERS ENGINEERING, INC.  
P.O. BOX 27, 260 S. MAIN ST., SUITE A  
SUGAR GROVE, OHIO 43155 (740) 746-0250

TSTECH AMERICA CROSS DOCK IMPROVEMENT

6630 TAYLOR ROAD SW  
PATASKALA, LICKING COUNTY, OHIO

EROSION & SEDIMENT CONTROL NOTES/DEATILS

CONTRACT JOB NO.: CW-18016

DRAWN BY: RAB SCALE: AS NOTED

CHECKED BY: JAS DATE: 05/21/2019

APPROVED BY: RAB SHEET: 15 OF 17

CROSSING  
WATERS  
ENGINEERING,

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NO.	REVISION	BY	DATE	RECORD	DRAWING



Species	Common	DBH (inches)	Alive	Bat_Tree	Tree_number
<i>Quercus phellos</i>	Pin oak	13	Yes	No	6
<i>Picea obies</i>	Norway spruce	9	Yes	No	12
<i>Picea obies</i>	Norway spruce	11	Yes	No	13
<i>Picea obies</i>	Norway spruce	14	Yes	No	15
<i>Picea obies</i>	Norway spruce	12.5	Yes	No	15
<i>Picea obies</i>	Norway spruce	11	Yes	No	16
<i>Picea obies</i>	Norway spruce	11.5	Yes	No	18
<i>Acer negundo</i>	Box elder	11.5	Yes	No	20
<i>Carya glabra</i>	Shagbark hickory	8	Yes	No	23
<i>Quercus dohrlandi</i>	Cottontwood	13.5	Yes	No	24
<i>Picea obies</i>	Norway spruce	10.5	Yes	No	26
<i>Picea obies</i>	Norway spruce	10.5	Yes	No	27
<i>Picea obies</i>	Norway spruce	9.5	Yes	No	28
<i>Picea obies</i>	Norway spruce	10	Yes	No	29
<i>Populus deltoides</i>	Cottontwood	10	Yes	No	30
<i>Fraxinus pennsylvanica</i>	Green ash	8	Yes	Yes	32
<i>Picea obies</i>	Norway spruce	8	Yes	No	35
<i>Picea obies</i>	Norway spruce	8	Yes	No	36
<i>Picea obies</i>	Norway spruce	13.5	Yes	No	37
<i>Picea obies</i>	Norway spruce	8.5	Yes	No	39
<i>Liriodendron tulipifera</i>	Tuliptree	11	Yes	No	40
<i>Quercus phellos</i>	Pin oak	14	Yes	No	42
<i>Quercus nigra</i>	Black walnut	11	Yes	No	43
<i>Liriodendron tulipifera</i>	Tuliptree	8	Yes	No	44
<i>Pinus strobus</i>	White pine	8.5	Yes	No	46
<i>Morus alba</i>	White mulberry	11.5	Yes	Yes	49
<i>Carya ovata</i>	Shagbark hickory	14.5	Yes	Yes	51
<i>Morus alba</i>	White mulberry	9	Yes	No	56
<i>Morus alba</i>	White mulberry	8.5	Yes	No	57
<i>Liriodendron tulipifera</i>	Tuliptree	8.5	Yes	No	59
<i>Pinus strobus</i>	White pine	12	Yes	No	60
<i>Pinus strobus</i>	White pine	13	Yes	No	61
<i>Pinus strobus</i>	White pine	15	Yes	No	62
<i>Pinus strobus</i>	White pine	12	Yes	No	64
<i>Pinus strobus</i>	White pine	12.5	Yes	No	65
<i>Populus deltoides</i>	Cottontwood	10	Yes	No	66
<i>Pinus strobus</i>	White pine	12	Yes	No	67
<i>Pinus strobus</i>	White pine	10	Yes	No	68
<i>Pinus strobus</i>	White pine	12	Yes	No	69
<i>Pinus strobus</i>	White pine	12	Yes	No	70
<i>Pinus strobus</i>	White pine	12	Yes	No	71
<i>Pinus strobus</i>	White pine	12	Yes	No	73
<i>Pinus strobus</i>	White pine	12	Yes	No	74
<i>Pinus strobus</i>	White pine	12	Yes	No	75
<i>Pinus strobus</i>	White pine	13.5	Yes	No	77
<i>Pinus strobus</i>	White pine	15	Yes	No	78

Species	Common	DBH (inches)	Alive	Brt. Tree	Tree number
<i>Picea abies</i>	Norway spruce	18	Yes	No	11
<i>Acer negundo</i>	Box elder	21	Yes	No	17
<i>Picea abies</i>	Norway spruce	20	Yes	No	19
<i>Pinus strobus</i>	White red-bay	22	Yes	Yes	21
<i>Alnus alba</i>	White alder	16	Yes	No	33
<i>Picea abies</i>	Norway spruce	18	Yes	No	34
<i>Liriodendron tulipifera</i>	Tulip tree	19	Yes	No	45
<i>Pinus strobus</i>	White pine	16	Yes	No	47
<i>Pinus strobus</i>	White pine	17	Yes	No	48
<i>Pinus strobus</i>	White pine	42.5	Yes	No	50
<i>Liriodendron tulipifera</i>	Tulip tree	17	Yes	No	52
<i>Liriodendron tulipifera</i>	Tulip tree	21	Yes	No	55
<i>Liriodendron tulipifera</i>	Tulip tree	19	Yes	Yes	63
<i>Liriodendron tulipifera</i>	Tulip tree	36.5	Yes	No	72
<i>Pinus strobus</i>	White pine	19	Yes	No	86
<i>Pinus strobus</i>	White pine	17	Yes	No	90
<i>Populus deltoides</i>	Cottonwood	22	Yes	No	93
<i>Pinus strobus</i>	White pine	22	Yes	No	99
<i>Pinus strobus</i>	White pine	17	Yes	No	101
<i>Quercus palustris</i>	Pin oak	20	Yes	No	104
<i>Pinus strobus</i>	Green ash	17.5	Yes	No	111
<i>Liriodendron tulipifera</i>	Tulip tree	16.5	Yes	No	114
<i>Pinus strobus</i>	White pine	16	Yes	No	132
<i>Pinus strobus</i>	Green ash	17.5	Yes	No	133
<i>Pinus strobus</i>	White pine	22	Yes	No	134
<i>Quercus palustris</i>	Pin oak	19	Yes	No	137
<i>Liriodendron tulipifera</i>	Tulip tree	16	Yes	No	139
<i>Pinus strobus</i>	White pine	19.5	Yes	No	140
<i>Pinus strobus</i>	White pine	16	Yes	No	143
<i>Pinus strobus</i>	White pine	18	Yes	No	144
<i>Liriodendron tulipifera</i>	Tulip tree	29	Yes	No	146
<i>Pinus strobus</i>	White pine	21	Yes	No	151
<i>Pinus strobus</i>	White pine	17	Yes	No	158
<i>Pinus strobus</i>	White pine	16	Yes	No	157
<i>Pinus strobus</i>	White pine	15	Yes	No	158
<i>Pinus strobus</i>	White pine	18	Yes	No	161

<i>Pinus strobus</i>	White pine	10.5	Yes	No	79
<i>Pinus strobus</i>	White pine	8.5	Yes	No	80
<i>Pinus strobus</i>	White pine	8	Yes	No	81
<i>Pinus strobus</i>	White pine	10	Yes	No	82
<i>Pinus strobus</i>	White pine	12.5	Yes	No	83
<i>Pinus strobus</i>	White pine	11.5	Yes	No	84
<i>Pinus strobus</i>	White pine	10.5	Yes	No	85
<i>Pinus strobus</i>	White pine	10.5	Yes	No	87
<i>Pinus strobus</i>	White pine	10	Yes	Yes	88
<i>Pinus strobus</i>	White pine	11	Yes	No	91
<i>Pinus strobus</i>	White pine	8	Yes	No	98
<i>Pinus strobus</i>	White pine	9.5	Yes	No	100
<i>Pinus strobus</i>	White pine	14	Yes	No	102
<i>Pinus strobus</i>	White pine	10.5	Yes	No	103
<i>Pinus strobus</i>	White pine	11	Yes	No	105
<i>Pinus strobus</i>	White pine	8.5	Yes	No	106
<i>Pinus strobus</i>	White pine	9	Yes	No	107
<i>Pinus strobus</i>	White pine	12	Yes	No	108
<i>Pinus strobus</i>	White pine	10	Yes	No	109
<i>Pinus strobus</i>	White pine	8	Yes	No	110
<i>Pinus strobus</i>	White pine	10.5	Yes	No	112
<i>Pinus strobus</i>	White pine	10.5	Yes	No	113
<i>Liriodendron tulipifera</i>	Tuliptree	11	Yes	No	116
<i>Pinus strobus</i>	White pine	13	Yes	No	118
<i>Pinus strobus</i>	White pine	8	Yes	No	119
<i>Pinus strobus</i>	White pine	13	Yes	No	120
<i>Fraxinus pennsylvanica</i>	Green ash	14	No	Yes	121
<i>Liriodendron tulipifera</i>	Tuliptree	9	Yes	No	123
<i>Pinus strobus</i>	White pine	8.5	Yes	No	124
<i>Pinus strobus</i>	White pine	9	Yes	No	126
<i>Pinus strobus</i>	White pine	11	Yes	No	127
<i>Liriodendron tulipifera</i>	Tuliptree	11	Yes	No	128
<i>Liriodendron tulipifera</i>	Tuliptree	13	Yes	No	129
<i>Pinus strobus</i>	White pine	15	Yes	No	131
<i>Pinus strobus</i>	White pine	9	Yes	No	135
<i>Pinus strobus</i>	Green ash	12.5	Yes	No	136
<i>Pinus strobus</i>	White pine	14	Yes	No	138
<i>Pinus strobus</i>	White pine	13	Yes	No	142
<i>Pinus strobus</i>	White pine	14	Yes	No	145
<i>Quercus palustris</i>	Pin oak	8	Yes	No	148
<i>Quercus palustris</i>	Pin oak	9.5	Yes	No	149
<i>Juglans nigra</i>	Black walnut	13	Yes	No	150
<i>Populus deltoides</i>	Cottonwood	14	Yes	No	154
<i>Quercus palustris</i>	Pin oak	13	Yes	No	155
<i>Quercus palustris</i>	Pin oak	8	Yes	No	156
<i>Pinus strobus</i>	White pine	9	Yes	No	159
<i>Juglans nigra</i>	Black walnut	12	Yes	No	172

TOTAL TREE REPLACEMENT = 330

Species	Common	DBH (inches)	Above	Bat	Tree	Tree number
<i>Picea abies</i>	Norway spruce	11	Yes	No	1	
<i>Picea abies</i>	Norway spruce	12.5	yes	No	3	
<i>Picea abies</i>	Norway spruce	9.5	Yes	No	4	
<i>Picea abies</i>	Norway spruce	11	Yes	No	5	
<i>Picea abies</i>	Norway spruce	16.5	Yes	No	7	
<i>Picea abies</i>	Norway spruce	17	Yes	No	8	
<i>Picea abies</i>	Norway spruce	8	Yes	No	9	
<i>Prunus serotina</i>	Black cherry	10.5	Yes	Yes	10	
<i>Rosaux pennsylvanica</i>	Green ash	12.5	No	No	92	
<i>Fraxinus pennsylvanica</i>	Green ash	12	No	Yes	94	
<i>Acer negundo</i>	Box elder	12	Yes	No	95	
<i>Morus alba</i>	White mulberry	23	Yes	No	96	
<i>Morus alba</i>	White mulberry	21	Yes	No	162	
<i>Morus alba</i>	White mulberry	1.6	Yes	No	163	
<i>Acer saccharum</i>	Sugar maple	8	Yes	No	164	
<i>Acer saccharinum</i>	Silver maple	44	Yes	No	165	
<i>Morus alba</i>	White mulberry	9.5	Yes	No	166	
<i>Populus deltoides</i>	Cottonwood	16	Yes	No	167	
<i>Populus deltoides</i>	Cottonwood	20	Yes	No	168	
<i>Populus deltoides</i>	Cottonwood	11.5	Yes	No	169	
<i>Populus deltoides</i>	Cottonwood	8	Yes	No	170	
<i>Populus deltoides</i>	Cottonwood	11	Yes	No	171	
<i>Acer saccharinum</i>	Silver maple	28	Yes	No	173	
<i>Populus deltoides</i>	Cottonwood	6	Yes	No	174	
<i>Acer negundo</i>	Box elder	9.5	Yes	No	175	
<i>Acer negundo</i>	Box elder	9.5	Yes	No	176	

 = EXISTING TREE TO PRESERVE  
 = EXISTING TREE TO REMOVE



**CROSSING  
WATERS  
ENGINEERING,**

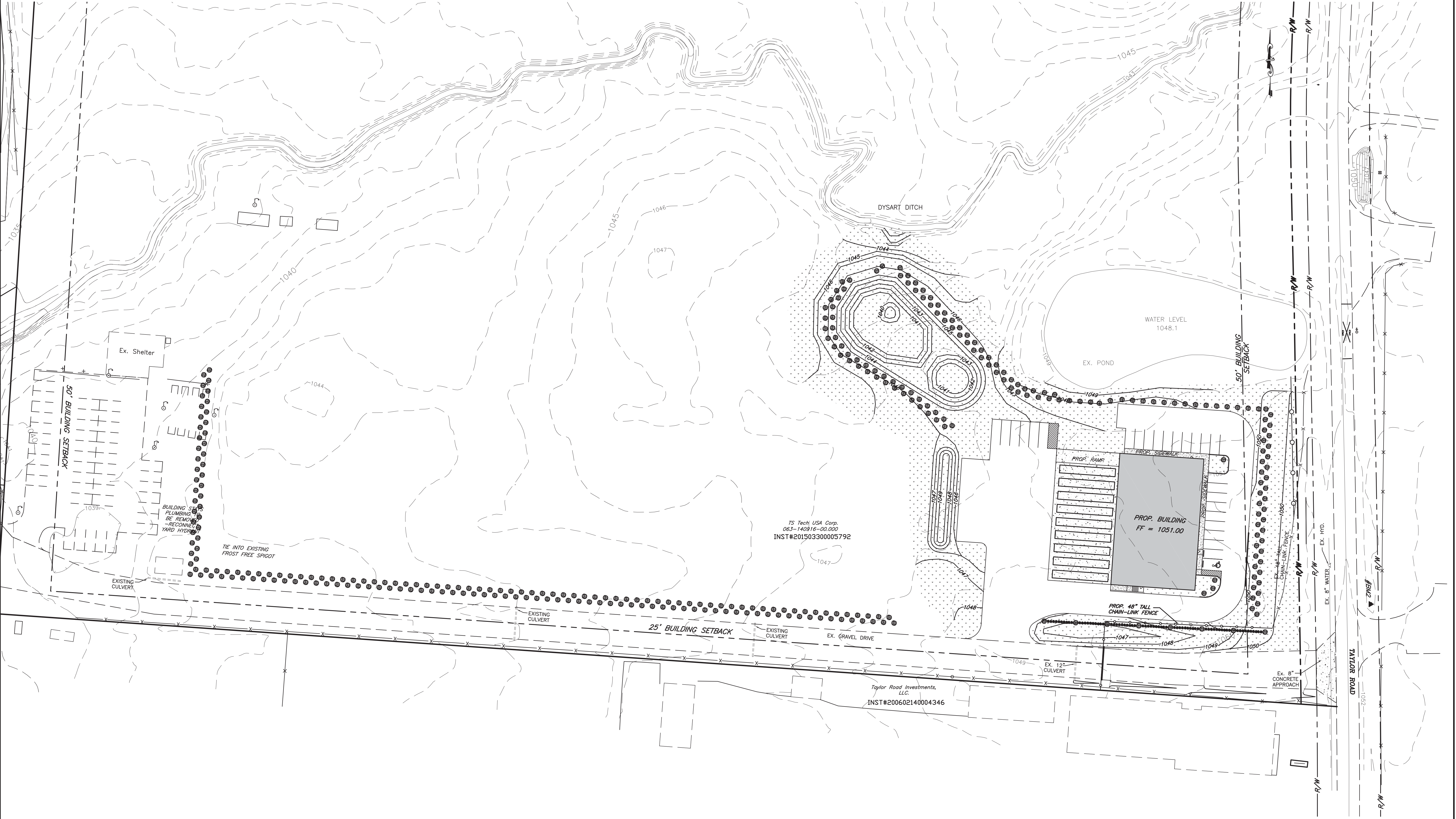
P.O. Box 27  
260 S. Main St., Suite A  
Sugar Grove, Ohio 43155  
(740) 746-0250  
[www.crossingwaterseng.com](http://www.crossingwaterseng.com)

NO.	REVISION	DATE	RECORD DRAWING
			BY:
			DATE:
			CONTRACTOR:
			CONSTRUCTED:
			NOTES:



6630 TAYLOR ROAD SW  
PATASKALA, LICKING COUNTY, OHIO  
TREE REMOVAL/PRESERVATION PLAN

CONTRACT	JOB NO.: CW-18016
DRAWN BY: RAB	SCALE: 1" = 40'
CHECKED BY: JAS	DATE: 05/21/2019
APPROVED BY: RAB	SHEET: 16 OF 17



- = AREAS TO BE PERMANENTLY SEEDED & MULCHED
- = TREES TO BE PLANTED IN ACCORDANCE WITH TABLE 1283.03-02 (2.5 IN MIN. CALIPER)  
TOTAL TREE REPLACEMENT = 330
- = SHRUBS TO BE PLANTED (3-FT HEIGHT & 95% OPACITY YEAR ROUND)



P.O. Box 27  
260 S. Main St., Suite A  
Sugar Grove, Ohio 43155  
(740) 746-0250  
www.crossingwaterseng.com



NO.	REVISION	BY	DATE	RECORD DRAWING

CROSSING WATERS ENGINEERING, INC. P.O. BOX 27, 260 S. MAIN ST., SUITE A SUGAR GROVE, OHIO 43155 (740) 746-0250	
TSTECH AMERICA CROSS DOCK IMPROVEMENT	
6630 TAYLOR ROAD SW PATASKALA, LICKING COUNTY, OHIO	
LANDSCAPING PLAN	
CONTRACT	JOB NO.: CW-18016
DRAWN BY: RAB	SCALE: 1" = 40'
CHECKED BY: JAS	DATE: 05/21/2019
APPROVED BY: RAB	SHEET: 17 OF 17

**From:** [Bruce Brooks](#)  
**To:** [Scott Fulton](#)  
**Subject:** TS Tech Exit  
**Date:** Wednesday, April 7, 2021 10:31:19 AM

---

Scott,

While at the Council meeting Monday, I overheard the conversation about TS Tech's attempt to use their lane attached to Taylor Road as an exit again. I wanted to throw my two cents in. I still have the same concerns as the last time we met on this matter. Close proximity with the elevated railroad tracks makes it somewhat hidden. Add in that there is an elevated volume of traffic from kids leaving school going that way and it makes it more dangerous. We get more than our share of speed complaints on Taylor due partly to school traffic. Now I have a greater concern that I heard they want to use it as a truck exit. In my opinion we should minimize truck traffic there not add to it. Also in my opinion, there isn't enough room for trucks to safely exit that lane without causing a greater slow to the traffic flow. I find it hard to believe that a truck could safely leave that driveway and maintain one lane to exit. This will cause accidents. If you wish to discuss this further, please don't hesitate to call or email me.

Thanks, Bruce

Chief Bruce Brooks #14  
Pataskala Division of Police  
623 West Broad Street  
Pataskala, Ohio 43062  
740-927-5701

**From:** [Philip Wagner](#)  
**To:** [Jack Kuntzman](#); [Steven Blake](#); [Jim Roberts](#); [Bruce Brooks](#); [Doug White](#); [Chris Gilcher](#); [Alan Haines](#); [Scott Fulton](#); [Scott Haines](#)  
**Subject:** RE: BZA Review Memo for 04-13-2021  
**Date:** Thursday, March 25, 2021 10:16:40 PM

---

**CAUTION: This email message came from an external (non-city) email account. Do not click on any links within the message or attachments to the message unless you recognize the sender's email account and trust the content.**

Jack,

As we understand matters, there are two requests of the Heights School District as follows:

**AP-21-001 – TS Tech is appealing to the Board their denial of an application for a driveway permit that would give them an access point on Taylor Road**

**VA-21-016 – Tractor Supply is requesting a variance to construct an 8' tall fence that exceeds the 6' code.**

Regarding TS Tech's request, I have heard concerns from community members regarding increased traffic on Taylor Road as well as traffic concerns with the intersection of Taylor and Broad. If you want to further discuss matters, please let me know.

Specific to Tractor Supply's request, the school district does not have a specific concern.

Thanks,

Philip Wagner

*Philip H. Wagner*, Ph.D.

Superintendent

Licking Heights Local Schools

***Honoring our legacy. Inspiring the present. Ready for the future.***

6539 Summit Road, S.W.

Pataskala, Ohio 43062

[www.lhschools.org](http://www.lhschools.org)

---

**From:** Jack Kuntzman <[jkuntzman@ci.pataskala.oh.us](mailto:jkuntzman@ci.pataskala.oh.us)>

**Sent:** Tuesday, March 16, 2021 3:28 PM

**To:** Steven Blake <[sblake@ci.pataskala.oh.us](mailto:sblake@ci.pataskala.oh.us)>; Jim Roberts <[jroberts@hullinc.com](mailto:jroberts@hullinc.com)>; Bruce Brooks <[bbrooks@pataskalapolicenet.net](mailto:bbrooks@pataskalapolicenet.net)>; Doug White <[DWhite@westlickingfire.org](mailto:DWhite@westlickingfire.org)>; Philip Wagner <[pwagner@lhschools.org](mailto:pwagner@lhschools.org)>; Chris Gilcher <[cgilcher@swlcws.com](mailto:cgilcher@swlcws.com)>; Alan Haines <[ahaines@ci.pataskala.oh.us](mailto:ahaines@ci.pataskala.oh.us)>; Scott Fulton <[sfulton@ci.pataskala.oh.us](mailto:sfulton@ci.pataskala.oh.us)>; Scott Haines <[shaines@hullinc.com](mailto:shaines@hullinc.com)>

**Subject:** BZA Review Memo for 04-13-2021

**Importance:** High

Good Afternoon Everyone,

You are receiving this email because one or more of the Applications submitted for the April 13, 2021 Board of Zoning Appeals is within your jurisdiction. Please see the list below for which Applications are being submitted for your review.

**VA-21-016:** Steven Blake, Jim Roberts, Bruce Brooks, Doug White, Philip Wagner, CJ Gilcher, Alan Haines.

**AP-21-001:** Steven Blake, Jim Roberts, Bruce Brooks, Doug White, Philip Wagner, CJ Gilcher, Alan Haines

If you have any comments or concerns regarding these applications, please have them submitted to me no later than April 2<sup>nd</sup>. They will be included in the Staff Report that is given to the Board members.

Here is a link to download the review memo:

[https://pataskala-my.sharepoint.com/:f:/g/personal/jkuntzman\\_ci\\_pataskala\\_oh\\_us/Eijfpr00pXXKtcH78BiqJg4BraDnJkMYZo9n-tsZc\\_BdmQ?e=sennoH](https://pataskala-my.sharepoint.com/:f:/g/personal/jkuntzman_ci_pataskala_oh_us/Eijfpr00pXXKtcH78BiqJg4BraDnJkMYZo9n-tsZc_BdmQ?e=sennoH)

**JACK R. KUNTZMAN**

City Planner

City of Pataskala

621 West Broad Street, Suite 2-A

Pataskala, Ohio 43062

C. Darcy Jalandoni  
djalandoni@porterwright.com

Porter Wright  
Morris & Arthur LLP  
One South Main Street  
Suite 1600  
Dayton, OH 45402

Direct: 614.227.2024  
Fax: 614.227.2100  
Main: 614.227.2000

[www.porterwright.com](http://www.porterwright.com)

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CLEVELAND  
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DAYTON  
NAPLES  
PITTSBURGH  
WASHINGTON, DC

March 9, 2021

VIA FEDERAL EXPRESS

Board of Zoning Appeals  
Attn: Clerk of Board of Zoning Appeals  
621 W. Broad Street  
Pataskala, OH 43062

Re: TS Tech Corporation v. City of Pataskala  
Property Location: 6630 Taylor Road SW, Pataskala, Ohio  
43068  
Parcel ID: 063-140916-00.00

Dear Sir/Madam:

Enclosed please find an original and two copies of the Notice of Appeal from Administrative Officer Decision for the City of Pataskala to be filed with your office. A return envelope is enclosed for your use in returning a date-stamped copy of the Notice.

Since we were unable to confirm whether there is a filing fee, please include an invoice for any such filing fee.

If you have any questions or need any further information, please do not hesitate to let me know. Thank you.

Very truly yours,



C. Darcy Jalandoni

CDC:car  
Enclosures

cc: Scott Fulton (via Federal Express)  
Brian Zets (via Federal Express)  
Cassandra Rice (via electronic mail)  
Ryan Sherman (via electronic mail)  
Jack Beeler (via electronic mail)  
Erin Wiggins (via electronic mail)

**BOARD OF ZONING APPEALS  
CITY OF PATASKALA  
621 W. Broad Street, Pataskala, Ohio 43062**

CITY OF PATASKALA

2021 MAR 10 PM 2:54

**IN THE MATTER OF:**

Case No. AP-21-001  
Application No. 21-051

**TS TECH USA CORPORATION,  
8400 East Broad Street  
Reynoldsburg, Ohio 43068**

**NOTICE OF APPEAL FROM  
ADMINISTRATIVE OFFICER  
DECISION FOR THE CITY OF  
PATASKALA**

Applicant,

vs.

*Property Location:* 6630 Taylor Road SW  
Pataskala, Ohio 43068

**CITY OF PATASKALA  
621 W. Broad Street  
Pataskala, Ohio 43062**

*Parcel ID:* 063-140916-00.00

*Date:* March 9, 2021

- 
1. Now comes TS TECH USA CORPORATION ("TS Tech" or "Applicant"), by and through undersigned counsel, and files this Notice of Appeal from the denial of its Application for Zoning Permit No. 21-051 (aka, Application for Driveway Permit, hereinafter "Application"). A copy of the Application is attached hereto as **Exhibit A**. The Application is made pursuant to City of Pataskala Permit Application 16-356, and the reservation of right made in Permit No. 19-252 which was approved by the City, specifically to request that the conditions imposed on the driveway as set forth in Permit No. 16-356 be removed and the private driveway be upgraded to provide full access to public roadway Taylor Road SW.
  2. The foregoing appeal is brought pursuant to the Pataskala Codified Ordinances (PCO), Chapter 1209.02, 1209.05, 1211.01 and 1211.03, which provide as follows:

## **1209.02 ZONING PERMITS REQUIRED**

No owner \* \* \* shall use or permit the use of any structure, building or land or part thereof, hereafter created, erected, changed, converted or altered, wholly or partly, until the Zoning Inspector issues a zoning permit showing that the building, structure, land or part, shall be issued only in conformity with the provisions of this Zoning Ordinance.

## **1209.05 APPROVAL OF ZONING PERMIT**

Within 30 days after the receipt of an application, the Zoning Inspector, or his/her designated agent, shall either approve or disapprove the application in conformance with the provisions of this code.

\* \* \*

In the case of disapproval, the Zoning Inspector shall state on the returned application the specific reasons for disapproval.

\* \* \*

In this case, the applicant may appeal to the Board of Zoning Appeals for a decision regarding the permit application, pursuant to Chapter 1211.

## **1211.01 POWERS OF ZONING INSPECTOR, BOARD OF ZONING APPEALS, AND CITY COUNCIL ON MATTERS OF APPEAL**

\* \* \*

The Board of Zoning Appeals shall:

(a) Review and decide appeals on decision or orders made by the Zoning Inspector or other administrative officers governing zoning a building...

Such appeal shall be taken within thirty days after the decision, by filing with the Clerk for the Board of Zoning Appeals a notice of appeal specifying the grounds thereof. The Clerk shall forthwith transmit to the Board all the papers constituting the record upon which the action appealed from was taken.

## **1211.03 APPEALS**

Appeals to the Board of Zoning Appeals concerning interpretation or administration of this Code may be taken by any person aggrieved including a tenant, or by a governmental officer, department, board, or bureau. Such appeal shall be taken within thirty days after the date of the decision, by filing with the Clerk for the Board of Zoning Appeals, a notice of appeal specifying the decision of the Zoning Inspector upon which the appeal is being taken. The Clerk shall transmit to the Board of Zoning Appeals all the papers constituting the record upon which the action appealed from was taken. The Clerk shall, in addition to transmitting the record to the Board, provide a summary description of the matter to the

City Administrator in order for the City to determine whether comment by the City as contemplated by Section 1211.01 is considered appropriate.

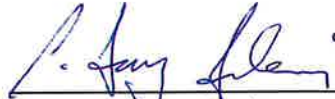
3. Applicant states that its Application was received by the Zoning Department on January 27, 2021.
4. On or about February 25, 2021, the Zoning Inspector returned Applicant's Application marked "Disapproved." A hand-written note in the Conditions section states: "Disapproved on 2-25-21. See attached."
5. Attached to Applicant's application purports to be an email sent from Alan Haines, the City of Pataskala's Public Service Director, to Scott Fulton, which was sent on or about February 25, 2021 at 2:08pm. It states as follows:

"Good afternoon. Regarding TS Tech's driveway, the attached email sent on November 26, 2019, is a thorough explanation of my observations and concerns with the proposed opening of the TS Tech access point on Taylor Road. My understanding is that neither conditions, nor the proposed usage, has changed substantially; therefore, my concerns still stand as stated. Without TS Tech taking any further action to address these concerns, I am still of the opinion that the drive should remain emergency access only. Let me know if you have any questions."
6. Applicant states that on or about December 2, 2020, December 23, 2020 and January 4, 2021, TS Tech attempted to engage in meaningful discussions with City personnel, including Scott Fulton ("Fulton") and Tim Hickin, about the concerns raised in Alan Haines's November 26, 2019 email.
7. In his December 23, 2020 email, Fulton stated "it is not necessary to propose a physical change in the existing driveway . . you can use the Driveway Permit Application to request the emergency access designation be altered and upgraded to full access." (See, Exhibit A, pg. 56.) Fulton further requested TS Tech to "clearly identify how the drive will be utilized" and to address the "several concerns" raised in Alan Haines's November 26, 2019 email.

8. In response, TS Tech specifically identified that the proposed application would remove all conditions imposed by the Application Permit No. 16-356 for unrestricted ingress and egress; that this proposed use did not require trucks to perform any illegal or improper maneuvers pursuant to the Ohio Commercial Driver License Manual and relevant laws and regulations; and the updated 2020 Traffic Study showed that the proposed use does not degrade traffic conditions on Taylor Road. (*See*, Exhibit A, pgs. 55-56.) TS Tech requested that City personnel specifically identify relevant zoning and traffic ordinances which are violated upon the proposed use, or identify improvements that would support the proposed usage.
9. Rather than respond to this request or suggest improvements to the driveway, the City directed Applicant to file its application.
10. Applicant alleges that the denial is wrongful, is not supported by the Pataskala Codified Ordinances or the Ohio Revised Code, and is unconstitutional, illegal, arbitrary, capricious, unreasonable, and/or unsupported by the preponderance of substantial, reliable, and probative evidence on the record.
7. Applicant demands that this matter be set for a hearing, to take place no later than forty-five (45) days from the date of this Appeal, pursuant to Chapter 1211.09 of the PCO.

**WHEREFORE**, Applicant APPEALS from the February 25, 2021 denial, and requests the Board set this matter for a hearing pursuant to Pataskala Codified Ordinance Chapter 1211.09.

Respectfully Submitted,



C. Darcy Jalandoni (0086981)  
PORTER WRIGHT MORRIS & ARTHUR LLP  
41 S. High Street, Ste. 2800-3200  
Columbus, OH 43215  
T: (614) 227-2024  
F: (614) 227-2100  
E: [djalandoni@porterwright.com](mailto:djalandoni@porterwright.com)

-and-

Cassandra Andres Rice (0090102)  
PORTER WRIGHT MORRIS & ARTHUR LLP  
One South Main Street, Ste. 1600  
Dayton, Ohio 45402  
T: (937) 449-6713  
F: (937) 449-6820  
E: [crice@porterwright.com](mailto:crice@porterwright.com)

# EXHIBIT A

## CITY OF PATASKALA PLANNING & ZONING DEPARTMENT

621 West Broad Street, Suite 2A

Pataskala, Ohio 43062



### DRIVEWAY PERMIT APPLICATION

(Pataskala Codified Ordinances Sections 1121.13 and 1291.02)

Property Information	
Address: 6630 Taylor Road SW	
Parcel Number: 063-140916-00.000	
Zoning: M-1, Manufacturing	Acres: 21.01

Applicant Information		
Name: TS Tech USA Corporation by Erin Wiggins - General Counsel		
Address: 8400 East Broad Street		
City: Reynoldsburg	State: Ohio	Zip: 43068
Phone: 614-322-4052	Email: erin.wiggins@tstech.com	

Property Owner Information		
Name: TS Tech USA Corporation		
Address: 8400 East Broad Street		
City: Reynoldsburg	State: Ohio	Zip: 43068
Phone: 614-322-4052	Email: erin.wiggins@tstech.com	

Staff Use
Application Number: 21-051
Fee: \$30.00
Filing Date: 1/27/2021
Receipt Number: 21665

Documents
<input checked="" type="checkbox"/> Application
<input checked="" type="checkbox"/> Fee
<input checked="" type="checkbox"/> Site Plan

Driveway Information
Describe the Project (Materials, dimensions, distance to property lines, etc.)
Pursuant to Permit Application 16-356 and the reservation of rights in Permit Application 19-252, Applicant requests full access to and use of the private driveway and public roadway. Please see supplemental attachment.

Documents to Submit
Driveway Application: Submit one (1) copy of the Driveway Permit Application.
Fee: Application fee of \$30.00
Site Plan: Submit one (1) copy of a site plan showing the following:
<ul style="list-style-type: none"><li>Location of all existing buildings and structures</li><li>Driveway location and dimensions</li><li>Driveway distance to property lines</li><li>Property lines, easements and right-of-way</li></ul>



### Important Information

**Applicability:** A Driveway Permit is required for new driveways or the expansion of an existing driveway.

**Driveway Regulations:** All driveways must meet the following regulations:

- Driveways must be graded and maintained so water does not accumulate or drain onto an adjacent property.
- Driveways cannot encroach onto an adjacent property.
- Driveways must have a minimum width of 10 feet as measured from the curb to the nearest portion of the garage.
- Additional regulations for driveways in Platted Subdivisions:
  - The driveway must have a maximum grade of 10 percent.
  - The driveway and apron must be located at least three (3) feet from the side lot line.
  - The apron must be three (3) feet wider than the driveway on each side.
  - The driveway must hard-surfaced with asphalt, concrete or pavers.
  - A concrete driveway must have a minimum depth of four (4) inches.
- It is the property owner's responsibility to determine property lines and certify the driveway meets the appropriate setbacks at the time of installation or construction.

#### Home Owners Association:

- It is important to check with the neighborhood Home Owners Association, if applicable, for any driveway requirements they may have.
- Driveways may require approval from the Home Owners Association prior to construction.

#### Public Service Department:

- A Right-of-Way permit may be required by the Public Service Department if work is to be done within the public right-of-way.
- Please contact the Public Service Department at 740-927-0145 for additional information.

#### Inspections:

- The Pataskala Planning and Zoning Department will inspect driveways constructed of concrete in platted subdivisions twice:
  1. First inspection once the driveway forms are placed.
  2. Second inspection upon completion to ensure that the driveway complies with the approved permit.
- All other driveways will be inspected by the Pataskala Planning and Zoning Department upon completion to ensure that the driveway complies with the approved permit.
- To schedule an inspection please contact the Zoning Inspector at 740-927-3885.

### Signatures

I certify the facts, statements and information provided on and attached to this application are true and correct to the best of my knowledge and understand the requirements for a driveway. Also, I authorize City of Pataskala staff to inspect the property as necessary as it pertains to this application.

Applicant (Required):

*Erin O'Sullivan*

Date:

1/29/2021

Property Owner (Required):

*James Irvine*

Date:

1 / 28 / 2021

### Zoning Inspector

☐ Approved

☒ Disapproved

PZC/BZA Application Number:

N/A

Zoning Inspector:

*Att He*

Approval Date:

Expiration Date:

Conditions:

Disapproved on 2-25-21. See attached

## **Scott Fulton**

---

**From:** Alan Haines  
**Sent:** Thursday, February 25, 2021 2:08 PM  
**To:** Scott Fulton  
**Subject:** TS Tech  
**Attachments:** TS Tech - Taylor Road and Cypress St.

Scott,

Good afternoon. Regarding TS Tech's driveway, the attached email sent on November 26, 2019, is a thorough explanation of my observations and concerns with the proposed opening of the TS Tech access point on Taylor Road. My understanding is that neither conditions, nor the proposed usage, has changed substantially; therefore, my concerns still stand as stated. Without TS Tech taking any further action to address these concerns, I am still of the opinion that the drive should remain emergency access only. Let me know if you have any questions.

Regards,

Alan W. Haines, P.E.  
Public Service Director  
City of Pataskala

621 W. Broad St.  
Suite 2B  
Pataskala, Ohio 43062

Office: 740-927-0145  
Cell: 614-746-5365  
Fax: 740-927-0228

## Scott Fulton

---

**From:** Alan Haines  
**Sent:** Tuesday, November 26, 2019 4:50 PM  
**To:** erin.wiggins@tstech.com  
**Cc:** BJ King; Scott Haines; Scott Fulton; Jim Roberts; Brian M. Zets (bzets@isaacwiles.com); crice@porterwright.com; Melissa Carter; Michael Powell; Suzanne Hayes  
**Subject:** TS Tech - Taylor Road and Cypress St.

Erin,

Good afternoon. I did go out last week to review the area with regard to the request to open this entrance for trucks and/or commuters. I have made the following observations:

1. Truck Traffic

a. Dimensions

- i. 120' = Distance between centerline of Cypress St. and TS Tech Drive
- ii. 70-80' = Average length of a Semi and trailer
- iii. 11' = Taylor Road lane width

b. Turning radius

i. Cypress St.

1. Inbound

- a. Cypress St. has a large radius on the SE corner allowing truck traffic to stay in-lane through the turn from Taylor Road northbound to Cypress St. eastbound.
- b. No truck traffic from the north.

2. Outbound

- a. All existing truck traffic must turn left (south)
  - i. No radius issues.
- b. Proposed truck traffic would turn right (north)
  - i. Turning radius combined with roadway width does not appear to be adequate for trucks to make a right-turn from Cypress St. onto Taylor Road and stay in their own lane.

ii. TS Tech Drive

1. Inbound

- a. All truck traffic from the south.
- b. No radius issues

2. Outbound

- a. Turning radius combined with roadway width does not appear to be adequate for trucks to make a right-turn out of this drive onto Taylor Road and stay in their own lane.

c. Concerns

- i. Trucks will not be able to enter Taylor Road south from the TS Tech Drive without crossing into the northbound lane.
- ii. Trucks will not be able to enter Taylor Road north from the Cypress St. without crossing into the southbound lane.
- iii. Trucks trying to make this maneuver will block normal traffic flow on Taylor Road.

d. Potential Solutions

i. TS Tech Drive

1. Increase turning radius on TS Tech Drive

- a. Driveway is near property line, so extending the radius might not be an option.

2. Stripe the driveway to channel trucks over far enough from the edge of the drive, so that they have an adequate radius to maintain their lane, and not cross into opposing traffic on Taylor Road.

- ii. Cypress St.
  - 1. Increase turning radius on Cypress St. to Taylor Road (north)
    - a. Existing public and private utilities would likely need relocated.
- iii. Taylor Road
  - 1. Install a two-way left-turn lane
    - a. With the short distance between the TS Tech drive and Cypress St, it does not appear that there will be enough distance for a truck to make it completely into a center lane; therefore, this option does not appear to address the issue.
- e. Conclusions
  - i. With the short distance between the TS Tech drive and Cypress St., it is clear that the existing conditions will not allow for trucks to safely enter/exit Taylor Road to go from TS Tech to Cypress St. and back.
  - ii. While a combination of the above solutions may enable trucks to stay within their lane while making the proposed movement, none of them adequately address the interruption to existing traffic that will be caused by the proposed truck movement; unless,
  - iii. If it can be shown that a truck can fully enter into a two-way left-turn lane between the drives, such that a truck can sit entirely in this lane without blocking north and southbound traffic while waiting to turn left, details of this option could be considered.
  - iv. Note that if any of the above options are shown to be acceptable to the City, any required improvements would be at the cost of TS Tech.

## 2. Commuter Traffic

- a. Traffic Study/Gap Analysis
  - i. While the traffic study shows that there will not be a reduced level of service, I still have a concern about available gaps in existing traffic that I do not believe has been addressed.
  - ii. Per the traffic study, 51 vehicles will be making this left-hand turn during the pm peak, with an average delay of 17 seconds. This delay accounts for all of these cars exiting over the course of the peak hour; however, if all of these vehicles are leaving at once, which is presumed to be true since it will be at shift change, the actual delay will be significantly larger than is represented in the study.
- b. Concerns
  - i. The concern is that with several vehicles using this exit for a left-turn onto Taylor Road all at once, and during times of peak traffic, that there will not be adequate gaps in the existing traffic to allow these vehicles to enter Taylor Road in a safe and timely manner. Essentially, my estimation is that this exit will stack up with vehicles waiting to get onto Taylor Road, wait times in the queue to get out will be long, and drivers will accept smaller gaps in traffic than what is safe due to being impatient, and accidents will result.
- c. Potential Solutions
  - i. Taylor Road
    - 1. Install a two-way left-turn lane
      - a. I had previously offered that this would allow drivers making a left-turn to cross southbound traffic when space is available, enter the center lane, and merge with northbound traffic when space is available; however, I have since been informed that this is not a legal movement.
    - 2. Install flashing yellow lights
      - a. I believe that this was suggested by TS Tech representation, but studies have shown that these are not effective; therefore, this is not a viable option.
- d. Conclusion
  - i. Without some traffic control, or other measure, to address this concern, opening this exit to outbound traffic is not in the best interest of the City.

As discussed, this topic will be on the agenda for the Street Committee meeting that will be held on December 2<sup>nd</sup> at 5:30. Let me know if questions.

Regards,

**Alan W. Haines, P.E.**  
**Public Service Director**  
**City of Pataskala**

**621 W. Broad Street**  
**Suite 2B**  
**Pataskala, Ohio 43062**

**Office: 740-927-0145**  
**Cell: 614-746-5365**  
**Fax: 740-927-0228**

**NOT APPROVED**

*Att Jlc 2-25-21  
#21-051*

**SUPPLEMENT to Driveway Permit Application  
City of Pataskala, Ohio  
Planning & Zoning Department**

**IN THE MATTER OF:**

**APPLICATION NO:** \_\_\_\_\_

**TS TECH USA CORPORATION**  
Applicant

Address: 6630 Taylor Road SW  
Parcel Number: 063-140916-00.000

**Introduction and Summary**

This Supplement to Driveway Permit Application is being provided in support of the Driveway Permit Application (the Application together with all supporting materials filed in connection therewith is referred to as the "Application") filed by TS Tech USA Corporation, as Applicant (the "Applicant" or "TS Tech"), pertaining to the 21.01 acres of land West of and adjacent to Taylor Road SW (the "Property"). The Property is in the M-1, Light Manufacturing zoning district, and it consists of developed land upon which sits a facility supporting operations of a manufacturing facility located in Jefferson Township.

Pursuant to Pataskala Codified Ordinances ("PCO") Chapters 1251 and 1291, the Application seeks the removal of conditions placed by City of Pataskala Permit No. 16-356 on a private drive (the "Private Drive") located on the Property. There is no rezoning or variance required with this application, as the proposed use of the Private Drive is permitted under the applicable sections or articles of the Zoning Code.

**Permitted Uses**

The Property is located in a M-1, Light Manufacturing zoning district. PCO Chapter 1251 governing Light Manufacturing districts generally permits development of manufacturing establishments which are clean and quiet. The uses identified below are permitted under the Chapter:

- (1) Manufacturing operations within the M-1 District which operate entirely within enclosed structures and generate minimum industrial traffic. *See* PCO 1251.01.
- (2) Parking and loading, as regulated in Chapter 1291. *See* PCO 1251.05(E)
- (3) Clearly marked entrances and exits to parking areas meeting the requirements of PCO 1291.15.

The City of Pataskala has already approved the construction of a cross-dock facility located on the Property, and has determined that the manufacturing operations proposed with such application fall within the Permitted Uses of the property, as a M-1 district. As a result, TS Tech requests the conditions imposed by Permit No. 16-356 be removed without improvements, and the Private Drive upgraded to full access.

### **Basis of Application**

An elemental right of ownership of a parcel of real property is the right of the owner to access abutting public roadways. "An owner of property abutting on a public highway possesses, as a matter of law, not only the right to the use of the highway in common with other members of the public, but also a private *right or easement* for the purpose of ingress and egress to and from his property, which latter right may not be taken away or destroyed or substantially impaired without compensation therefor." (Emphasis added.) *State ex rel. Merritt v. Linzell*, 163 Ohio St. 97, 56 Ohio Op. 166, 126 N.E. 2d 53 (1955); *State ex rel. New Wen, Inc. v. Marchbanks*, 2020-Ohio-63, ¶ 16.

TS Tech, being the owner of the Property, is entitled to access Taylor Road SW by way of the Private Drive; indeed, if TS Tech did not own the adjacent parcel of land, this would be the only means of accessing public roadways from the Property. *See State ex rel. McKay v. Kauer*, 156 Ohio St. 347, 46 Ohio Op. 204, 102 N.E.2d 703 (1951) (finding that depriving a property owner of access to one of two abutting public roadways constituted an taking); *State ex rel. OTR v. City of Columbus*, 76 Ohio St.3d 203, 211 (1996) ("The law in Ohio is clear. An owner of a parcel of real property has a right to access public streets or highways on which the property abuts.") The drawings demonstrate that the Property complies with Chapters 1251.01, *et seq.*, and further demonstrates that the width of the private drive exceeds the minimum requirements set by PCO 1291.15. Indeed, TS Tech agrees to assist the City of Pataskala with installation of additional traffic regulatory measures analogous to the needs of the intersection.

In support of this Application, TS Tech is also including an updated traffic study conducted by MS Consultants. **The traffic study demonstrates that lifting the conditions of the permit will not degrade traffic operations on Taylor Road SW, and would provide an improvement in traffic operations for traffic signals on Broad Street, especially the Broad Street/Taylor Road SW intersection.** Further, TS Tech notes that the Board of Zoning Appeals very recently approved Conditional Use Application CU-20-003, allowing the nearby property at 6747 Taylor Road SW to be used as a semi-truck storage facility—a use which would likely cause a greater increase in traffic operations than those at issue here.

Upgrading the Private Drive to full access is also a permitted use, since the Private Drive complies with the minimum requirements for access drives set by Pataskala Codified Ordinance section 1291.15. The Cross-Dock will continue to meet all requirements of section 1251.01, *et seq.*, including side and front yard setbacks. Additionally, since the City of Pataskala awarded Permit No. 16-356 and approved construction of the cross-dock facility on the Property, the Private Drive has been paved with a hard-surfaced material (asphalt) that will not generate dust or dirt when in use.

### **Previous Application**

In September 2020, TS Tech submitted a Driveway Permit Application seeking the same relief as sought in the present application. The City of Pataskala returned the Application, stating it was improperly submitted. TS Tech appealed that denial to the Pataskala Board of Zoning Appeals ("BZA"). A hearing was held by the BZA on November 11, 2020. During the hearing, the City of Pataskala asserted that TS Tech waived its right to change and/or appeal the conditions imposed by Permit No. 16-356, and further argued that a Driveway Permit Application was not the appropriate avenue for the relief sought by TS Tech but asserted that other avenues were available. The BZA confirmed that TS Tech could seek a change in the conditions imposed and instructed TS Tech and the City of Pataskala to work together on the appropriate avenue. Subsequent to the BZA hearing, TS Tech and the City of Pataskala exchanged multiple emails on these topics. The City of Pataskala ultimately instructed TS Tech to file a new Driveway Permit Application requesting relief which is the reason for this filing. A copy of that email correspondence is included in this supplement.

### **Conclusion**

TS Tech seeks termination of all Conditions of Approval imposed by City of Pataskala Permit No. 16-356 and upgrade of the Private Drive to full access.





FOR THE CITY OF LOS ANGELES

**THE CONSTITUTION**

[illegible]

**INDEX**

ENGINE ROOM HOURS IN ACCORDANCE WITH THE CODED ORDINANCES OF THE CITY OF MILWAUKEE ARE 7 AM TO 3 PM MONDAY THROUGH FRIDAY AND 8 AM TO 4 PM ON WEDNESDAY. SMOKE ROOMS ARE NOT ACTIVATED WITHOUT PRIOR PERMISSION FROM THE MILWAUKEE PUBLIC SERVICE DIVISION. NO WORK SHALL BE PERMITTED ON SMOKE ROOMS ON MAJOR HOLIDAYS.

## STUDY OBJECTIVES

THE CONTRACTOR AND SUBCONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR COMPLYING WITH ALL FEDERAL, STATE AND LOCAL SAFETY REQUIREMENTS. TOGETHER WITH EXISTING SAFETY PRECAUTIONS AT ALL TIMES, THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF PERSONS (INCLUDING EMPLOYEES AND RESIDENTS), PROPERTY AND THE ENVIRONMENT. THE CONTRACTOR IS RESPONSIBLE FOR THE PROTECTION OF ALL UTILITIES AND STRUCTURES. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING AND NEWLY CONSTRUCTED AND SUBCONTRACTED STRUCTURES. THE CONTRACTOR AND SUBCONTRACTOR SHALL ALSO BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING AND NEWLY CONSTRUCTED AND SUBCONTRACTED STRUCTURES.

COMPLIANCE WITH THE OCCUPATIONAL SAFETY AND HEALTH ACT OF 1970 IS REQUIRED OF ALL CONTRACTORS

**CONTACT 1-800-225-2227**  
**OR 714/225-2227**

**UNITED STATES**  
DEPARTMENT OF THE INTERIOR  
BUREAU OF LAND MANAGEMENT  
WASHINGTON, D. C. 20250

THE FOLLOWING INFORMATION WAS OBTAINED FROM THE RECORDS OF THE NEW YORK STATE DEPARTMENT OF CORRECTIONS:

**RETRACING OF DUNE 171 AND OTHER SITES**

[illegible]

### Abstract

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UTILITIES AND STRUCTURES EXISTING UNDER OR ADJACENT TO THE PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UTILITIES AND STRUCTURES EXISTING UNDER OR ADJACENT TO THE PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UTILITIES AND STRUCTURES EXISTING UNDER OR ADJACENT TO THE PROJECT.

**1997-1998**

ALL PUBLIC CONSTRUCTION PROJECTS SHALL BE FUNDED BY FEDERAL, STATE, LOCAL, AND FEDERAL AID. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROVISION OF ALL MATERIALS, EQUIPMENT, AND LABOR REQUIRED FOR THE CONSTRUCTION OF THE PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROVISION OF ALL MATERIALS, EQUIPMENT, AND LABOR REQUIRED FOR THE CONSTRUCTION OF THE PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROVISION OF ALL MATERIALS, EQUIPMENT, AND LABOR REQUIRED FOR THE CONSTRUCTION OF THE PROJECT.

## STATE OF NEW YORK

[illegible]

**STUDY NUMBER: 101010001**

NO NON-PLANNED FLOOD VULNERABLE SHALL BE MOVED ON PUBLIC RIGHTS. CONDITIONS MAY BE GRANTED BY THE CITY OF ALBUQUERQUE, NEW MEXICO, AND OTHER AGENCIES. ANY AGENCIES GRANTING CONDITIONS MUST BE IN WRITING AND ANY CHANGE MUST BE APPROVED BY THE CONTRACTOR TO BE

**Abstract**

THE CONTRACTOR SHALL REFUSE TO ACCEPT ANY AND ALL OFFERING MORE DIRECTLY DURING OF THE NO IN  
DEFERRED OF THE CONTRACT AT HIS OWN RISK. ALL SAID MORE TO BE AFFIRMED OR ADJUSTED TO IN  
Satisfaction of the owner's dignity and the city of Alaska

ALL SOME FINANCE ISSUES, CHANGES STRUCTURES OF OTHER FINANCE, FINANCIAL THAT ARE TO REMAIN THE SAME. THE CONTRACT SHALL BE ASSIGNED TO THE PARTY OR ARE ENTITLED TO OWNED FINANCING MORE UNDER THE CONTRACT SHALL BE ASSIGNED TO THE PARTY OR ARE ENTITLED TO OWNED FINANCING MORE UNDER THE CONTRACT. THE COST OF A FINANCING MORE UNDER THE CONTRACT, UNLESS OTHERWISE PROVIDED IN THE CONTRACT, THE COST OF A FINANCING MORE UNDER THE CONTRACT, UNLESS OTHERWISE PROVIDED IN THE CONTRACT.

[illegible]

## HOW TO USE

**OFFICE OF THE ATTORNEY GENERAL**

## CONCLUSIONS

THE DEVELOPER'S CONTRACTOR SHALL MAINTAIN ONE (1) COMPLETE FIELD MAKEUP SET OF CONSTRUCTION DRAWINGS ON SITE FOR THE SOLE PURPOSE OF ATTENDING THE FOLLOWING AUTOMATION I. ANY MODIFICATIONS MADE TO THE APPROVED MANUFACTURING ASSEMBLY

THE FOLLOWING IS TO BE SUBMITTED TO THE CITY ENGINEER

© 1997 DALLAS, TX 75201

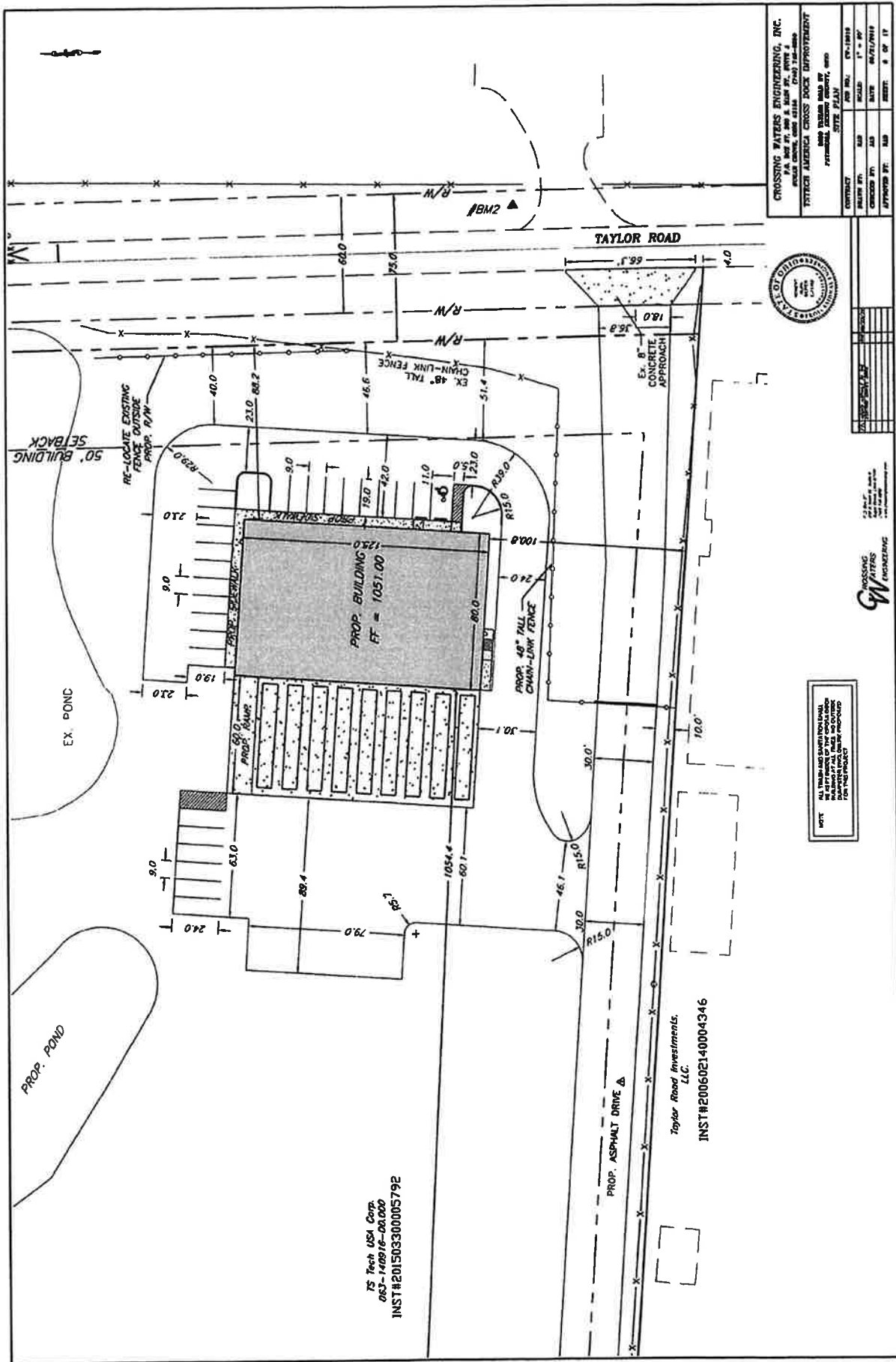


**ROSSING  
WATERS  
ENGINEERING**

[illegible]







**CROSSING WATERS ENGINEERING, INC.**  
1000 N. 10th St., Suite 100  
Fritch, Texas 79430  
Phone: (940) 346-1000  
Fax: (940) 346-1001  
Email: info@crossingwaters.com

**PROJECT:** TAYLOR ROAD CROSS-DOCK IMPROVEMENT  
SHEET: 8 OF 17

CONTRACT	NO. 0000000000	DATE	08/11/2015
CLIENT	Taylor Road Investments, LLC	DATE	08/11/2015
DESIGNED BY	WAT	DATE	08/11/2015
CHECKED BY	WAT	DATE	08/11/2015
APPROVED BY	WAT	DATE	08/11/2015



NOTE: ALL DIMENSIONS SHOWN ON THIS PLAN ARE BASED ON THE AS-BUILT CONDITIONS OF THE EXISTING IMPROVEMENTS. ANY CHANGES TO THE PLAN SHALL BE MADE BY A REVISION SHEET.

**CROSSING WATERS ENGINEERING**





TS TECH EXHIBIT A  
PAGE 18





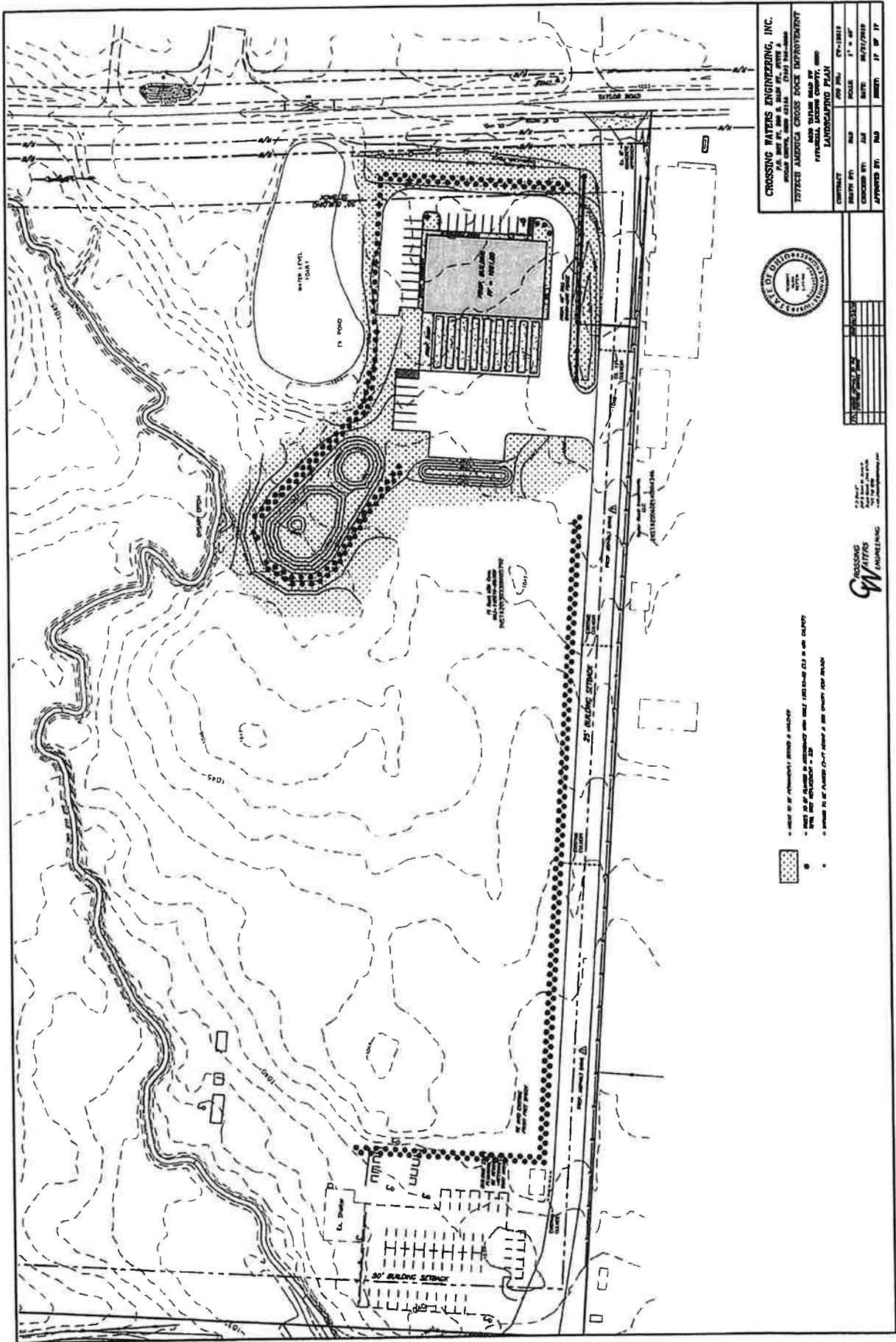
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# **TS Tech USA Traffic Access Study**

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Franklin County & Pataskala, Ohio

March 20, 2020



**ms consultants, inc.**  
engineers, architects, planners

TS TECH EXHIBIT A

PAGE 27

## **INTRODUCTION**

ms consultants has performed a traffic access study for the Taylor Road driveway at the TS Tech USA facility in Pataskala, Ohio. The purpose of this study is to determine the impact of allowing the TS Tech driveway onto Taylor Road to be used for exiting employee traffic during the PM peak hour. Figure 1 shows the study area and the TS Tech driveway.

## **EXISTING CONDITIONS**

TS Tech, USA is a manufacturing and assembly facility located on East Broad Street (SR 16) in Jefferson Township, Franklin County, Ohio. There is one access point, off E. Broad Street, for employees and visitors to enter and exit the site. This access point also serves an AT&T office facility. The access point intersects E. Broad Street at a signal, directly across from a Limited Brands corporate facility. The existing access point has a security checkpoint with guardhouse.

A driveway has been constructed between the TS Tech USA parking lot and Taylor Road. However, this driveway is gated and locked. Employee vehicles are not able to enter or exit the facility via this Taylor Road driveway. A small number of trucks per day use this driveway to cross Taylor Road, travelling between the TS Tech's new Cross Dock facility to a staging area on Cypress Lane. These are the only vehicles currently permitted to use the gated Taylor Road access point.

While the TS Tech, USA building is located in Jefferson Township, Franklin County, Ohio, the proposed eastern access driveway is located in the City of Pataskala. Taylor Road is a 2-lane collector roadway in the City of Pataskala, with a 35mph posted speed limit. No turn lanes are present on Taylor Road within the vicinity of the driveway. The Broad Street/Taylor Road intersection is signalized.

## **PROPOSED CONDITIONS**

TS Tech, USA has proposed to improve the eastern driveway to Taylor Road, such that employees could exit the site onto Taylor Road. Because of security concerns, only outbound/eastbound traffic would be permitted on the Taylor Road driveway. Because all vehicles are required to pass through security, allowing inbound traffic from Taylor Road would require a new guardhouse, gates, and security staff. Thus, TS Tech USA only desires to have outbound traffic onto Taylor Road.

If granted, the new outbound driveway to Taylor Road would only be open during the afternoon shift change period, approximately 3:00-4:30 PM. During the remainder of the day, the driveway would be gated and locked.

The proposed driveway location has adequate sight distance to meet the requirements specified in the Ohio Department of Transportation (ODOT) Location & Design Manual, Volume One. For a 35mph roadway like Taylor Road, a sight distance of 390' is necessary. Well over 500' of sight distance exists in both directions on Taylor Road.



**Figure 1: Study Area Map**  
Traffic Access Study  
TS Tech USA - Taylor Road Driveway  
me consultants, inc.  
engineers, architects, planners

## **TRAFFIC VOLUMES**

Turning movement counts were performed at the study area intersections on a typical weekday in March 2018 from 3:00-4:00 PM. This hour represents the PM peak dismissal hour of TS Tech USA employees at the end of first shift. At the E. Broad Street/TS Tech driveway, EMH&T conducted a turning movement count in February 2020, in conjunction with a future traffic impact study for a residential development. Thus, the 2020 count data for this intersection was used instead. The turning movement counts can be found in the Appendix.

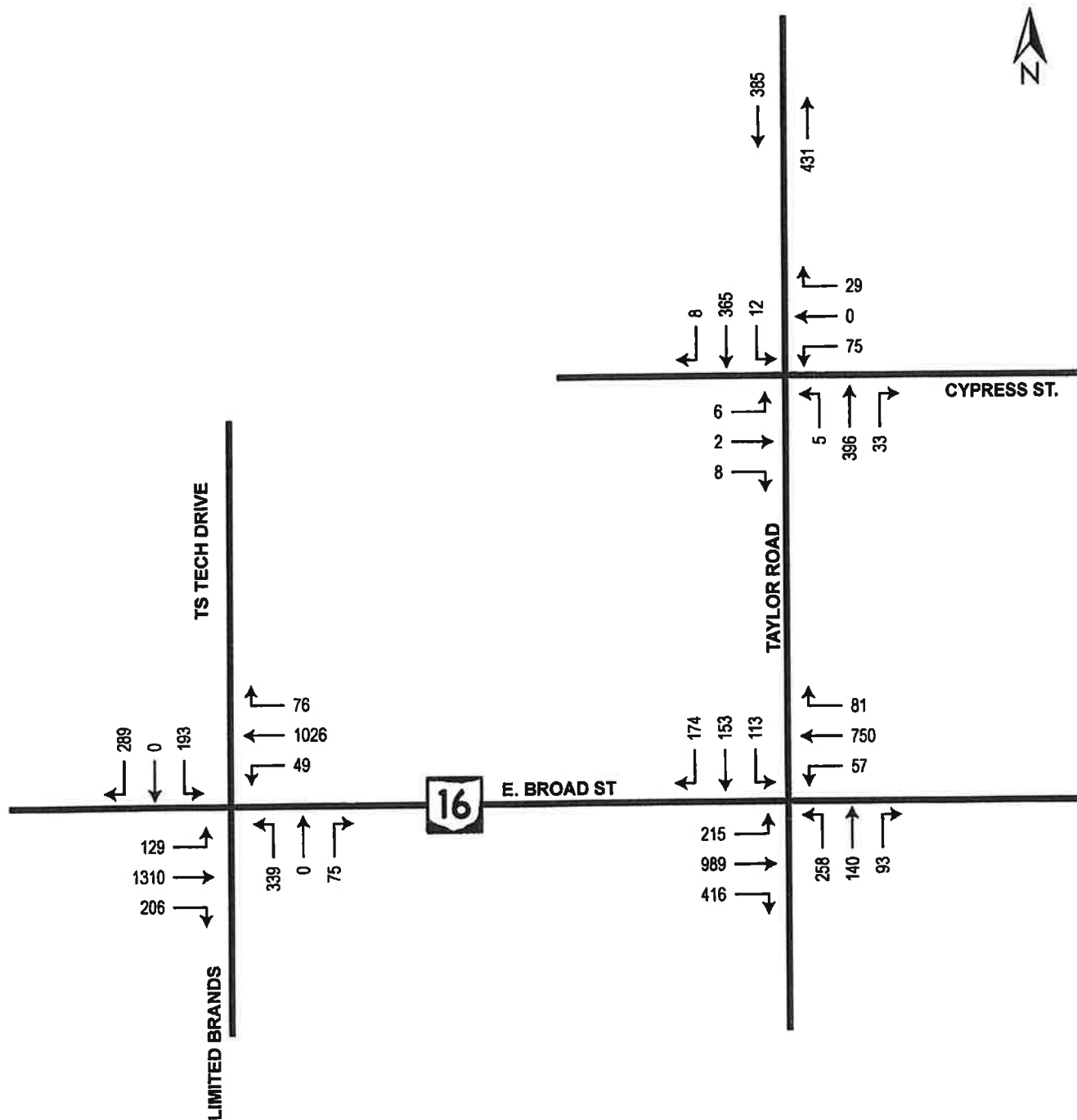
- E. Broad Street & Limited/TS Tech driveway
- E. Broad Street & Taylor Road
- Taylor Road & Cypress Street SW (used to determine the volume on Taylor Road at the proposed TS Tech USA driveway)

The traffic count volumes were increased to a Design Year (2038) by applying a 1.0% annual linear growth rate to the Broad Street and Taylor Road volumes. A 1.25 DHV factor was also applied to obtain design hourly volumes for the 2038 PM peak hour. It was also assumed that two trucks cross Taylor Road between the Cross Dock facility during the PM peak hour.

Traffic using the proposed Taylor Road driveway was estimated based on the outbound (southbound) left turns onto Broad Street from the traffic counts and information provided by TS Tech USA staff. It was assumed that 70% of the southbound left turns onto Broad Street would divert to the new driveway, if constructed. Drivers could avoid the congested signals on Broad Street by using the Taylor Road access point. The projected traffic volumes are shown on **Figure 2** and **Figure 3**.

## **CAPACITY ANALYSIS**

Capacity analyses of the study area intersections were performed using Synchro traffic modeling software (version 10). Appropriate peak hour factors were applied to the TS Tech hourly volumes, to account for the fact that the vast majority of the existing shift traffic exits the facility during a 30-minute window. For a suburban area such as this, a Level-of-Service (LOS) of D or better is typically considered acceptable. The results of the analysis are shown in **Table 1**:



**FIGURE 2: DESIGN YEAR (2038)  
PM PEAK HOUR VOLUMES  
NO-BUILD CONDITION**  
TS Tech USA Traffic Access Study



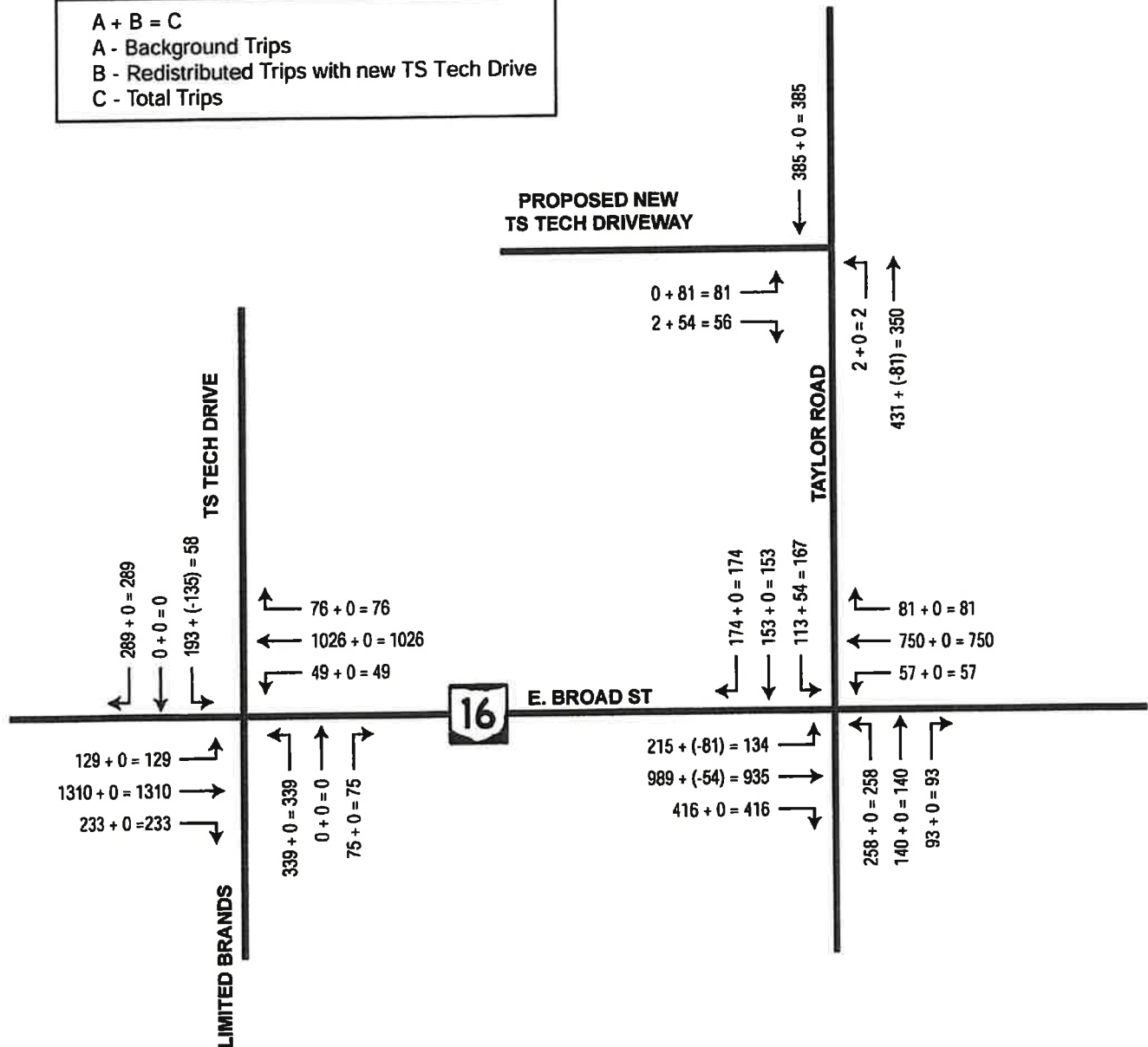


$$A + B = C$$

A - Background Trips

B - Redistributed Trips with new TS Tech Drive

C - Total Trips



**FIGURE 3: DESIGN YEAR (2038)  
PM PEAK HOUR VOLUMES**

**BUILD CONDITION**

TS Tech USA Traffic Access Study



**Table 1: Design Year PM Peak Capacity Analysis**

	Existing Configuration (No-Build condition)	With Taylor Road driveway (Build condition)
Taylor Road & TS Tech driveway	--	C* 18.7
E. Broad Street & Limited/TS Tech	F 102.9	F 103.7
E. Broad Street & Taylor Road	F 81.0	E 69.0

\*Unsignalized intersection. Delay reported is for the stopped approach.

The capacity analysis shows that the proposed Taylor Road access point would operate at an acceptable level-of-service. Taylor Road through traffic would not be affected or delayed, as it would remain a free-flow movement. The proposed driveway would help to reduce delays at the Broad Street/Taylor Road signal, which have significant delays and queuing today and will continue to worsen. The analysis indicates that this signal operations could improve from LOS F to LOS E with the use of the driveway by employees.

## TURN LANE WARRANTS

Because the proposed Taylor Road driveway would operate as outbound (eastbound) only, no vehicles shall turn into the TS Tech USA property from this location. Therefore, no turn lanes on Taylor Road would be warranted.

An exercise was performed to determine how many inbound turning vehicles would trigger turn lanes on Taylor Road, if TS Tech USA ever elected to construct a security gate and allow inbound traffic from Taylor Road. Turn lane warrants were performed in accordance with the ODOT State Highway Access Management Manual, section 4.5.

The turn lane warrant graphs show that turn lanes would not likely be warranted on Taylor Road until the following two conditions occurred:

- Inbound traffic was permitted at this driveway (which would require a new security guardhouse)
- Current employment increased by 50%

A copy of the warrant graphs can be found in the Appendix.

## TRUCK TRAFFIC

An AutoTurn analysis was performed for the truck traffic that moves across Taylor Road from the CrossDock facility and Cypress Lane. These trucks (WB-53 vehicles) turn right onto Taylor Road and then left into the facility and vice-versa.

The AutoTurn analysis indicates that right-turning trucks do intrude across the Taylor Road centerline. However, sufficient gaps in both directions of traffic exist for drivers to make these turns without conflicting or interfering with Taylor Road traffic. Based on a 12-second gap time for a tractor-trailer to make a right turn and equations provided in Section 402-3 of the ODOT Traffic Engineering Manual (TEM), there would be 56 two-way gaps long enough for a truck to complete a right-turning maneuver during the PM peak hour. In short, trucks would wait for an average of one minute to complete a right turn – which is very reasonable given the limited truck volume.

## **SUMMARY**

In summary, the proposed TS Tech USA driveway onto Taylor Road will not degrade traffic operations on Taylor Road. The proposed driveway would provide an improvement in traffic operations for traffic signals on Broad Street – notably the Broad Street/Taylor Road intersection. Adequate sight distance exists at the proposed driveway. No turn lanes are necessary for the driveway, as inbound TS Tech USA traffic will be prohibited from this location with the exception of a small number of trucks per day.

N:\03\60\10678-01 TS Tech\2020-03-17 TSTech-TrafficStudy.docx



**ms consultants, inc.**  
engineers, architects, planners

March 20, 2020  
Page 8

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# Appendix

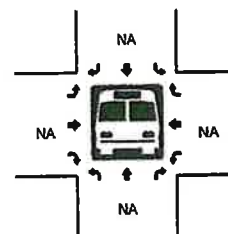
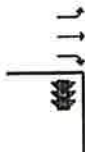
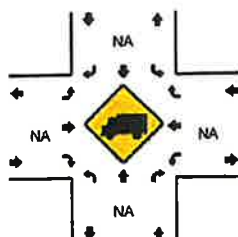
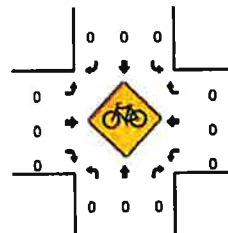
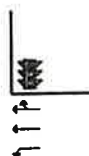
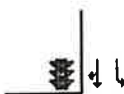
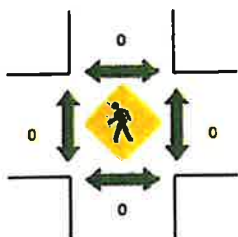
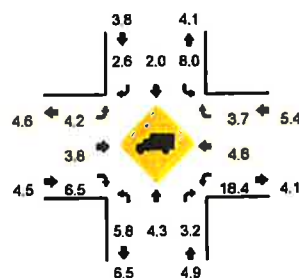
**EMH&T**  
**5500 New Albany Road**  
**Columbus, OH 43054**  
**emht.com**

File Name : E Broad St - TS Tech Drive  
 Site Code : 00000000  
 Start Date : 2/4/2020  
 Page No : 1

Groups Printed- Cars - Trucks - School Buses																				
Start Time	TSTECH Southbound					BROAD ST Westbound					L-BRANDS Northbound					BROAD ST Eastbound				
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total
02:45 PM	17	0	26	0	43	5	201	2	0	208	12	0	0	0	12	10	220	35	0	265
Total	17	0	26	0	43	5	201	2	0	208	12	0	0	0	12	10	220	35	0	265
03:00 PM	58	0	66	0	124	12	136	3	0	151	58	0	22	0	80	9	186	61	0	256
03:15 PM	62	0	95	0	157	17	141	9	1	168	32	0	6	0	38	22	246	61	0	329
03:30 PM	20	0	39	0	59	8	161	21	0	190	119	0	25	0	144	27	229	35	0	291
03:45 PM	14	0	31	0	45	2	146	28	0	176	62	0	7	0	69	45	212	8	0	265
Total	154	0	231	0	385	39	584	61	1	685	271	0	60	0	331	103	873	165	0	1141
04:00 PM	28	0	51	0	79	2	174	27	0	203	43	0	8	0	51	64	263	1	0	328
04:15 PM	26	0	35	0	61	2	169	23	0	194	65	0	11	0	76	58	261	6	0	325
04:30 PM	22	0	40	0	62	1	142	1	0	144	80	0	16	0	96	8	284	9	0	301
04:45 PM	15	0	30	0	45	1	196	5	0	202	113	0	16	0	129	2	244	5	0	251
Total	91	0	156	0	247	6	681	56	0	743	301	0	51	0	352	132	1052	21	0	1205
05:00 PM	14	0	24	0	38	2	163	1	0	166	175	0	30	0	205	2	238	6	0	246
Grand Total	276	0	437	0	713	52	1629	120	1	1802	759	0	141	0	900	247	2383	227	0	2857
Appreh %	38.7	0	61.3	0		2.9	90.4	6.7	0.1		84.3	0	15.7	0		8.6	83.4	7.9	0	
Total %	4.4	0	7	0	11.4	0.8	26	1.9	0	28.7	12.1	0	2.2	0	14.3	3.9	38	3.6	0	45.6
Cars	275	0	426	0	701	52	1544	119	1	1716	758	0	141	0	899	235	2320	227	0	2782
% Cars	99.6	0	97.5	0	98.3	100	94.8	99.2	100	95.2	99.9	0	100	0	99.9	95.1	97.4	100	0	97.4
Trucks	1	0	11	0	12	0	54	1	0	55	1	0	0	0	1	12	63	0	0	75
% Trucks	0.4	0	2.5	0	1.7	0	3.3	0.8	0	3.1	0.1	0	0	0	0.1	4.9	2.6	0	0	2.6
School Buses	0	0	0	0	0	0	31	0	0	31	0	0	0	0	0	0	0	0	0	0
% School Buses	0	0	0	0	0	0	1.9	0	0	1.7	0	0	0	0	0	0	0	0	0	0.5

### Method for determining peak hour: Total Entering Volume

QC JOB #: 14650603  
DATE: Tue, Mar 13 2018

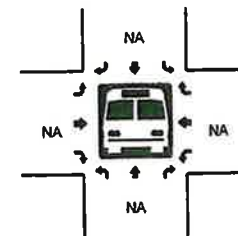
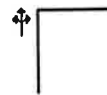
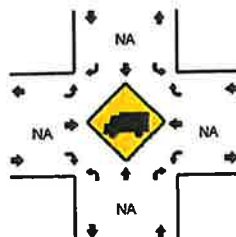
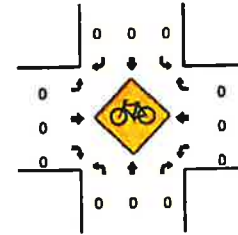
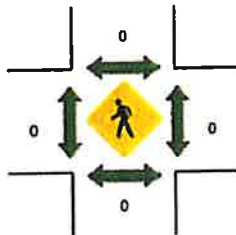
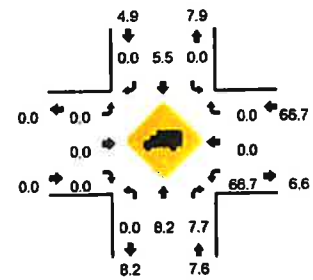
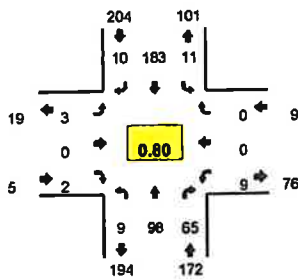


**Comments:**

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

Type of peak hour being reported: Intersection Peak

Method for determining peak hour: Total Entering Volume

LOCATION: Taylor Rd -- Cypress St SW  
CITY/STATE: Pataskala, OHQC JOB #: 14650604  
DATE: Tue, Mar 13 2018Peak-Hour: 6:00 AM -- 7:00 AM  
Peak 15-Min: 6:45 AM -- 7:00 AM

15-Min Count Period	Taylor Rd (Northbound)				Taylor Rd (Southbound)				Cypress St SW (Eastbound)				Cypress St SW (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
6:00 AM	1	16	10	0	1	37	1	0	0	0	0	0	1	0	0	0	67	
6:15 AM	0	22	7	0	1	39	2	0	1	0	0	0	2	0	0	0	80	
6:30 AM	1	35	27	0	1	50	4	0	0	0	0	0	3	0	0	0	121	
6:45 AM	1	25	21	0	8	57	3	0	2	0	2	0	3	0	0	0	122	390
Peak 15-Min Flowrates	4	100	84	0	32	228	12	0	8	0	8	0	12	0	0	0	488	
All Vehicles	4	100	84	0	32	228	12	0	8	0	8	0	12	0	0	0	488	
Heavy Trucks	0	4	12	0	0	28	0	0	0	0	0	0	8	0	0	0	52	
Pedestrians	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Bicycles	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Railroad	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Stopped Buses	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	

Comments:

Report generated on 3/15/2018 12:02 PM

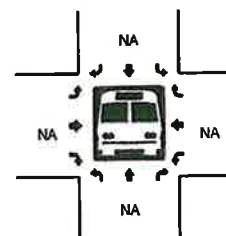
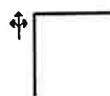
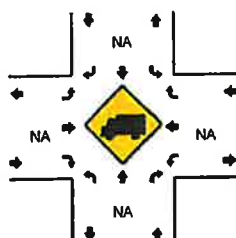
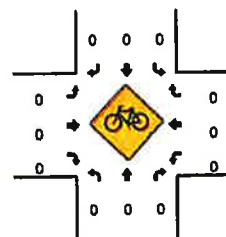
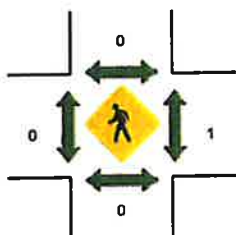
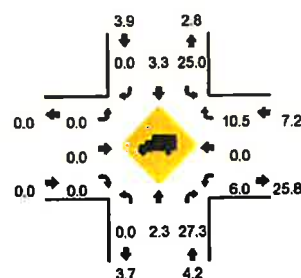
SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

TS TECH EXHIBIT A

PAGE 38

**Method for determining peak hour: Total Entering Volume**

QC JOB #: 14650601  
DATE: Tue, Mar 13 2018

**Comments:**

**SOURCE:** Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212



# OHIO DEPARTMENT OF TRANSPORTATION



## Transportation Data Management System

<a href="#">Home</a>	<a href="#">TMC</a>	<a href="#">TCLS</a>	<a href="#">TTDS</a>	<a href="#">PMS</a>	<a href="#">PMDS</a>	<a href="#">RSMS</a>	<a href="#">NMDS</a>	<a href="#">PMMS</a>	<a href="#">WOTS</a>	<a href="#">RTTV</a>
<a href="#">Login</a>										<a href="#">Email This</a>

List View All DIRs

Record	1	of 1	Goto Record	go
Location ID	754598	MPO ID		
Type	SPOT	HPMS ID		
On NHS		On HPMS		
LRS ID	TLICTR00169™C	LRS Loc Pt.	1.685	
SF Group	Urban Minor Arterial (4);Collector(5-6);Local(7)	Route Type	TR	
AF Group	URBAN_MINOR_ARTERIAL	Route	00169	
GF Group	URBAN_MINOR_ARTERIAL	Active	Yes	
Class Dist Grp		Category	Local	
Seas Clas Grp	Urban Minor Arterial (4);Collector(5-6);Local(7)			
WIM Group				
QC Group	Default			
Fnc'l Class	Minor Arterial	Milepost		
Located On	TAYLOR RD			
Loc On Alias				
More Detail				
STATION DATA				

Directions: 2-WAY

AADT								
	Year	AADT	DHV-30	K %	D %	PA	BC	Src
	2017	6,646 <sup>3</sup>						Grown from 2016
	2016	6,471 <sup>3</sup>						Grown from 2015
	2015	6,210 <sup>3</sup>						Grown from 2014
	2014	6,144 <sup>3</sup>						Grown from 2013
	2013	6,302						
1-5 of 6								
Travel Demand Model								



# OHIO DEPARTMENT OF TRANSPORTATION



## Transportation Data Management System

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Record [1](#) of 1 Goto Record [go](#)

Location ID	31925	MPO ID	
Type	SPOT	HPMS ID	000016011430
On NHS		On HPMS	Yes
LRS ID	SFRASR00016**C	LRS Loc Pt	12.3
SF Group	URBAN_OTHER_PRINCIPAL_ARTERIAL	Route Type	SR
AF Group	URBAN_OTHER_PRINCIPAL_ARTERIAL	Route	00016
GF Group	URBAN_OTHER_PRINCIPAL_ARTERIAL	Active	Yes
Class Dist Grp		Category	State Program
Seas Class Grp	URBAN_OTHER_PRINCIPAL_ARTERIAL		
WIM Group			
QC Group	Default		
Funct'l Class	Other Principal Arterial	Milepost	
Located On	E BROAD ST		
Loc On Alias			
	SR16 E OF WAGGONER RD, IN COLUMBUS		

More Detail [▶](#)

### STATION DATA

Directions: [2-WAY](#) [EB](#) [WB](#) [?](#)

[1](#) [2](#) [1](#) [2](#)

AADT <a href="#">?</a>							
	Year	AADT	DHV-30	K %	D %	PA	BC
	2017	26,205 <sup>3</sup>		9	60	24,577 (94%)	1,627 (6%)
	2016	26,205	2,262	9	60	24,577 (94%)	1,627 (6%)
	2015	29,976 <sup>3</sup>		10	56	28,123 (94%)	1,853 (6%)
	2014	29,429 <sup>3</sup>		10	56	27,610 (94%)	1,819 (6%)
	2013	29,224	2,852	10	56	27,417 (94%)	1,807 (6%)

[|<<](#) [<](#) [>](#) [>>|](#) 1-5 of 12

Travel Demand Model									
Model Year	Model AADT	AM PHV	AM PPV	MD PHV	MD PPV	PM PHV	PM PPV	NT PHV	NT PPV

VOLUME COUNT				VOLUME TREND <a href="#">?</a>	
	Date	Int	Total	Year	Annual Growth
<a href="#">↶</a>	Thu 12/1/2016	15	27,579	2017	0%
<a href="#">↶</a>	Wed 11/30/2016	15	27,531	2016	-13%
<a href="#">↶</a>	Thu 5/2/2013	60	34,723	2015	2%
<a href="#">↶</a>	Wed 5/1/2013	60	35,338	2014	1%



<a href="#">Home</a>	<a href="#">TMC</a>	<a href="#">TCLS</a>	<a href="#">TTDS</a>	<a href="#">PMS</a>	<a href="#">PMDS</a>	<a href="#">RSMS</a>	<a href="#">NMDS</a>	<a href="#">PMMS</a>	<a href="#">WOTS</a>	<a href="#">RTTV</a>
<a href="#">Back</a>	<a href="#">Login</a>									

## Volume Count Report

LOCATION INFO	
Location ID	31925
Type	SPOT
Funct'l Class	3
Located On	E BROAD ST
Direction	2-WAY
County	FRANKLIN
Community	COLUMBUS
MPO ID	
HPMS ID	000016011430
Agency	ODOT

COUNT DATA INFO	
Count Status	Accepted
Start Date	Wed 11/30/2016
End Date	Thu 12/1/2016
Start Time	12:00:00 AM
End Time	12:00:00 AM
Direction	
Notes	
Station	000319257200
Study	
Speed Limit	
Description	
Sensor Type	Tube Class

























INTERVAL: 15-MIN					
Time	15-min Interval				Hourly Count
	1st	2nd	3rd	4th	
0:00-1:00	69	69	54	45	237
1:00-2:00	57	25	54	39	175
2:00-3:00	70	47	43	42	202
3:00-4:00	61	55	86	74	276
4:00-5:00	43	52	71	96	262
5:00-6:00	91	149	204	223	667
6:00-7:00	261	371	356	381	1,369
7:00-8:00	457	457	441	436	1,791
8:00-9:00	420	423	422	415	1,680
9:00-10:00	407	318	298	301	1,324
10:00-11:00	322	298	323	326	1,269
11:00-12:00	342	355	357	352	1,406
12:00-13:00	389	365	390	428	1,572
13:00-14:00	388	362	403	427	1,580
14:00-15:00	451	420	506	464	1,841
15:00-16:00	473	481	538	555	2,047
16:00-17:00	503	555	543	552	2,153
17:00-18:00	536	536	527	511	2,110
18:00-19:00	449	486	436	398	1,769
19:00-20:00	348	337	329	320	1,334
20:00-21:00	317	232	241	231	1,021
21:00-22:00	222	200	155	129	706
22:00-23:00	139	92	110	92	433
23:00-24:00	90	75	57	85	307
Total					27,531
AADT					26,017
AM Peak	07:00-08:00				1,791
PM Peak	16:15-17:15				2,186

Count Navigation: << < > >>	Count Type: VOLUME ▼
-----------------------------	----------------------

Directions: 2-WAY ☐ EB ☐ WB ☐ ?

1 2 1 2



												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	129	1310	206	49	1026	76	339	0	75	193	0	289
Future Volume (veh/h)	129	1310	206	49	1026	76	339	0	75	193	0	289
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1856	1856	1856	1856	1856	1856	1856	1856	1856	1856	1856	1856
Adj Flow Rate, veh/h	152	1394	0	65	1091	99	547	0	123	371	0	545
Peak Hour Factor	0.85	0.94	1.00	0.75	0.94	0.77	0.62	0.92	0.61	0.52	0.92	0.53
Percent Heavy Veh, %	3	3	3	3	3	3	3	3	3	3	3	3
Cap, veh/h	192	1214		154	1055	96	495	0	324	401	0	454
Arrive On Green	0.07	0.34	0.00	0.05	0.32	0.32	0.14	0.00	0.21	0.23	0.00	0.29
Sat Flow, veh/h	1767	3526	1572	1767	3269	296	3428	0	1572	1767	0	1572
Grp Volume(v), veh/h	152	1394	0	65	588	602	547	0	123	371	0	545
Grp Sat Flow(s),veh/h/ln	1767	1763	1572	1767	1763	1802	1714	0	1572	1767	0	1572
Q Serve(g_s), s	6.7	40.5	0.0	2.8	38.0	38.0	17.0	0.0	7.9	24.2	0.0	34.0
Cycle Q Clear(g_c), s	6.7	40.5	0.0	2.8	38.0	38.0	17.0	0.0	7.9	24.2	0.0	34.0
Prop In Lane	1.00		1.00	1.00		0.16	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	192	1214		154	569	582	495	0	324	401	0	454
V/C Ratio(X)	0.79	1.15		0.42	1.03	1.03	1.10	0.00	0.38	0.92	0.00	1.20
Avail Cap(c_a), veh/h	226	1214		226	569	582	495	0	324	465	0	454
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	0.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	29.6	38.6	0.0	30.0	39.8	39.8	50.3	0.0	40.2	44.5	0.0	41.8
Incr Delay (d2), s/veh	15.0	76.7	0.0	1.8	46.5	46.5	72.1	0.0	0.7	22.5	0.0	109.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	3.4	29.3	0.0	1.2	22.8	23.3	12.3	0.0	3.2	13.1	0.0	27.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	44.6	115.2	0.0	31.8	86.4	86.4	122.5	0.0	41.0	67.0	0.0	151.3
LnGrp LOS	D	F		C	F	F	F	A	D	E	A	F
Approach Vol, veh/h		1546	A		1255			670			916	
Approach Delay, s/veh		108.3			83.6			107.5			117.2	
Approach LOS		F			F			F			F	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	11.2	45.5	22.0	39.0	13.7	43.0	31.7	29.3				
Change Period (Y+Rc), s	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0				
Max Green Setting (Gmax), s	11.0	38.0	17.0	34.0	11.0	38.0	31.0	20.0				
Max Q Clear Time (g_c+I1), s	4.8	42.5	19.0	36.0	8.7	40.0	26.2	9.9				
Green Ext Time (p_c), s	0.0	0.0	0.0	0.0	0.1	0.0	0.6	0.4				
<b>Intersection Summary</b>												
HCM 6th Ctrl Delay			102.9									
HCM 6th LOS			F									
<b>Notes</b>												
Unsignalized Delay for [EBR] is excluded from calculations of the approach delay and intersection delay.												

	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↰	↑	↱	↰	↑	↱	↰	↑	↱	↰	↑	↱
Traffic Volume (veh/h)	215	989	416	57	750	81	258	140	93	113	153	174
Future Volume (veh/h)	215	989	416	57	750	81	258	140	93	113	153	174
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No				No			No			No	
Adj Sat Flow, veh/h/ln	1856	1856	1856	1856	1856	1856	1856	1856	1856	1856	1856	1856
Adj Flow Rate, veh/h	299	1099	452	62	815	88	280	152	101	123	166	189
Peak Hour Factor	0.72	0.90	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	3	3	3	3	3	3	3	3	3	3	3	3
Cap, veh/h	394	879	745	154	1320	142	245	240	159	298	157	179
Arrive On Green	0.11	0.47	0.47	0.05	0.41	0.41	0.10	0.23	0.23	0.07	0.20	0.20
Sat Flow, veh/h	1767	1856	1572	1767	3209	347	1767	1040	691	1767	792	901
Grp Volume(v), veh/h	299	1099	452	62	448	455	280	0	253	123	0	355
Grp Sat Flow(s), veh/h/ln	1767	1856	1572	1767	1763	1793	1767	0	1731	1767	0	1693
Q Serve(g_s), s	10.8	55.0	24.6	2.3	23.3	23.3	12.0	0.0	15.3	6.3	0.0	23.0
Cycle Q Clear(g_c), s	10.8	55.0	24.6	2.3	23.3	23.3	12.0	0.0	15.3	6.3	0.0	23.0
Prop In Lane	1.00		1.00	1.00		0.19	1.00		0.40	1.00		0.53
Lane Grp Cap(c), veh/h	394	879	745	154	725	737	245	0	399	298	0	336
V/C Ratio(X)	0.76	1.25	0.61	0.40	0.62	0.62	1.14	0.00	0.63	0.41	0.00	1.06
Avail Cap(c_a), veh/h	511	879	745	214	725	737	245	0	399	355	0	336
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	19.9	30.5	22.5	27.1	27.0	27.0	35.8	0.0	40.3	33.9	0.0	46.5
Incr Delay (d2), s/veh	4.8	121.8	3.6	1.7	3.9	3.9	101.9	0.0	3.3	0.9	0.0	65.2
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	51.8	9.5	0.9	9.9	10.1	12.4	0.0	6.8	2.8	0.0	15.6	
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	24.8	152.4	26.2	28.8	30.9	30.8	137.7	0.0	43.5	34.8	0.0	111.7
LnGrp LOS	C	F	C	C	C	C	F	A	D	C	A	F
Approach Vol, veh/h		1850			965			533			478	
Approach Delay, s/veh		100.9			30.7			93.0			91.9	
Approach LOS		F			C			F			F	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	11.1	60.0	17.0	28.0	18.3	52.7	13.3	31.7				
Change Period (Y+Rc), s	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0				
Max Green Setting (Gmax), s	55.0	12.0	23.0	21.0	44.0	12.0	23.0					
Max Q Clear Time (g_c+1/4), s	57.0	14.0	25.0	12.8	25.3	8.3	17.3					
Green Ext Time (p_c), s	0.0	0.0	0.0	0.0	0.5	4.9	0.1	0.7				
Intersection Summary												
HCM 6th Ctrl Delay			81.0									
HCM 6th LOS			F									

Intersection						
Int Delay, s/veh	0					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↖			↗	↗	
Traffic Vol, veh/h	0	0	0	437	390	0
Future Vol, veh/h	0	0	0	437	390	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	0	0	0	475	424	0

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	899	424	424	0	-	0
Stage 1	424	-	-	-	-	-
Stage 2	475	-	-	-	-	-
Critical Hdwy	6.43	6.23	4.13	-	-	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.43	-	-	-	-	-
Follow-up Hdwy	3.527	3.327	2.227	-	-	-
Pot Cap-1 Maneuver	308	628	1130	-	-	-
Stage 1	658	-	-	-	-	-
Stage 2	624	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	308	628	1130	-	-	-
Mov Cap-2 Maneuver	308	-	-	-	-	-
Stage 1	658	-	-	-	-	-
Stage 2	624	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	0	0	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1130	-	-	-	-
HCM Lane V/C Ratio	-	-	-	-	-
HCM Control Delay (s)	0	-	0	-	-
HCM Lane LOS	A	-	A	-	-
HCM 95th %tile Q(veh)	0	-	-	-	-

	↖	→	↗	↖	←	↖	↖	↑	↗	↘	↓	↙
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↑↑	↗	↖	↑↑		↖	↑		↖	↑	
Traffic Volume (veh/h)	129	1310	206	49	1026	76	339	0	75	58	0	289
Future Volume (veh/h)	129	1310	206	49	1026	76	339	0	75	58	0	289
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00		1.00	1.00		1.00	1.00		1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1856	1856	1856	1856	1856	1856	1856	1856	1856	1856	1856	1856
Adj Flow Rate, veh/h	152	1394	0	65	1091	99	547	0	123	112	0	545
Peak Hour Factor	0.85	0.94	1.00	0.75	0.94	0.77	0.62	0.92	0.61	0.52	0.92	0.53
Percent Heavy Veh, %	3	3	3	3	3	3	3	3	3	3	3	3
Cap, veh/h	190	1233		153	1074	97	462	0	543	149	0	464
Arrive On Green	0.07	0.35	0.00	0.05	0.33	0.33	0.13	0.00	0.35	0.08	0.00	0.29
Sat Flow, veh/h	1767	3526	1572	1767	3269	296	3428	0	1572	1767	0	1572
Grp Volume(v), veh/h	152	1394	0	65	588	602	547	0	123	112	0	545
Grp Sat Flow(s),veh/h/ln	1767	1763	1572	1767	1763	1802	1714	0	1572	1767	0	1572
Q Serve(g_s), s	6.7	41.5	0.0	2.8	39.0	39.0	16.0	0.0	6.6	7.4	0.0	35.0
Cycle Q Clear(g_c), s	6.7	41.5	0.0	2.8	39.0	39.0	16.0	0.0	6.6	7.4	0.0	35.0
Prop In Lane	1.00		1.00	1.00		0.16	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	190	1233		153	579	592	462	0	543	149	0	464
V/C Ratio(X)	0.80	1.13		0.43	1.01	1.02	1.18	0.00	0.23	0.75	0.00	1.18
Avail Cap(c_a), veh/h	210	1233		210	579	592	462	0	543	313	0	464
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	0.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	29.6	38.6	0.0	30.0	39.8	39.8	51.3	0.0	27.6	53.1	0.0	41.8
Incr Delay (d2), s/veh	18.1	69.6	0.0	1.9	41.2	41.2	102.7	0.0	0.2	7.4	0.0	99.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	3.6	28.6	0.0	1.2	22.4	22.9	13.5	0.0	2.6	3.8	0.0	26.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	47.7	108.2	0.0	31.9	81.0	81.0	154.0	0.0	27.8	60.8	0.0	141.3
LnGrp LOS	D	F		C	F	F	F	A	C	E	A	F
Approach Vol, veh/h		1546	A		1255			670			657	
Approach Delay, s/veh		102.2			78.5			130.9			127.5	
Approach LOS		F			E			F			F	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	11.2	46.5	21.0	40.0	13.7	44.0	15.0	46.0				
Change Period (Y+Rc), s	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0				
Max Green Setting (Gmax), s	10.0	39.0	16.0	35.0	10.0	39.0	21.0	30.0				
Max Q Clear Time (g_c+I1), s	4.8	43.5	18.0	37.0	8.7	41.0	9.4	8.6				
Green Ext Time (p_c), s	0.0	0.0	0.0	0.0	0.0	0.0	0.2	0.7				

#### Intersection Summary

HCM 6th Ctrl Delay	103.7
HCM 6th LOS	F

#### Notes

Unsignalized Delay for [EBR] is excluded from calculations of the approach delay and intersection delay.

	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↰	↑	↱	↰	↑	↱	↰	↑	↱	↰	↑	↱
Traffic Volume (veh/h)	134	935	416	57	750	81	258	140	93	167	153	174
Future Volume (veh/h)	134	935	416	57	750	81	258	140	93	167	153	174
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1856	1856	1856	1856	1856	1856	1856	1856	1856	1856	1856	1856
Adj Flow Rate, veh/h	146	1016	452	62	815	88	280	152	101	217	166	189
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.77	0.92	0.92
Percent Heavy Veh, %	3	3	3	3	3	3	3	3	3	3	3	3
Cap, veh/h	338	872	739	153	1480	160	258	213	142	320	156	177
Arrive On Green	0.06	0.47	0.47	0.05	0.46	0.46	0.11	0.21	0.21	0.10	0.20	0.20
Sat Flow, veh/h	1767	1856	1572	1767	3209	347	1767	1040	691	1767	792	901
Grp Volume(v), veh/h	146	1016	452	62	448	455	280	0	253	217	0	355
Grp Sat Flow(s), veh/h/ln	1767	1856	1572	1767	1763	1793	1767	0	1731	1767	0	1693
Q Serve(g_s), s	5.0	55.0	25.0	2.1	21.5	21.5	13.0	0.0	15.9	11.5	0.0	23.0
Cycle Q Clear(g_c), s	5.0	55.0	25.0	2.1	21.5	21.5	13.0	0.0	15.9	11.5	0.0	23.0
Prop In Lane	1.00		1.00	1.00		0.19	1.00		0.40	1.00		0.53
Lane Grp Cap(c), veh/h	338	872	739	153	813	827	258	0	355	320	0	333
V/C Ratio(X)	0.43	1.17	0.61	0.40	0.55	0.55	1.09	0.00	0.71	0.68	0.00	1.07
Avail Cap(c_a), veh/h	383	872	739	212	813	827	258	0	355	320	0	333
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	17.2	31.0	23.1	26.8	22.8	22.8	36.1	0.0	43.3	34.0	0.0	47.0
Incr Delay (d2), s/veh	0.9	86.9	3.8	1.7	2.7	2.6	81.0	0.0	6.6	5.7	0.0	68.3
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	9	43.2	9.7	0.8	8.8	9.0	11.7	0.0	7.4	5.4	0.0	15.8
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	18.1	117.9	26.8	28.6	25.4	25.4	117.1	0.0	49.9	39.7	0.0	115.3
LnGrp LOS	B	F	C	C	C	C	F	A	D	D	A	F
Approach Vol, veh/h		1614			965		533				572	
Approach Delay, s/veh		83.4			25.6		85.2				86.6	
Approach LOS		F			C		F				F	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	11.1	60.0	18.0	28.0	12.1	59.0	17.0	29.0				
Change Period (Y+Rc), s	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0				
Max Green Setting (Gmax), s	54.0	13.0	23.0	10.0	54.0	12.0	24.0					
Max Q Clear Time (g_c+I1), s	57.0	15.0	25.0	7.0	23.5	13.5	17.9					
Green Ext Time (p_c), s	0.0	0.0	0.0	0.0	0.1	5.6	0.0	0.7				
Intersection Summary												
HCM 6th Ctrl Delay			69.0									
HCM 6th LOS			E									

TS Tech US - Revised Study (2020)  
4: Taylor Road & TS Tech Driveway

PM Peak - Build condition  
03/16/2020

Intersection

Int Delay, s/veh 4.8

Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↖	↗		↖	↗	
Traffic Vol, veh/h	81	56	2	385	350	0
Future Vol, veh/h	81	56	2	385	350	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	0	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	50	50	92	92	92	92
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	162	112	2	418	380	0

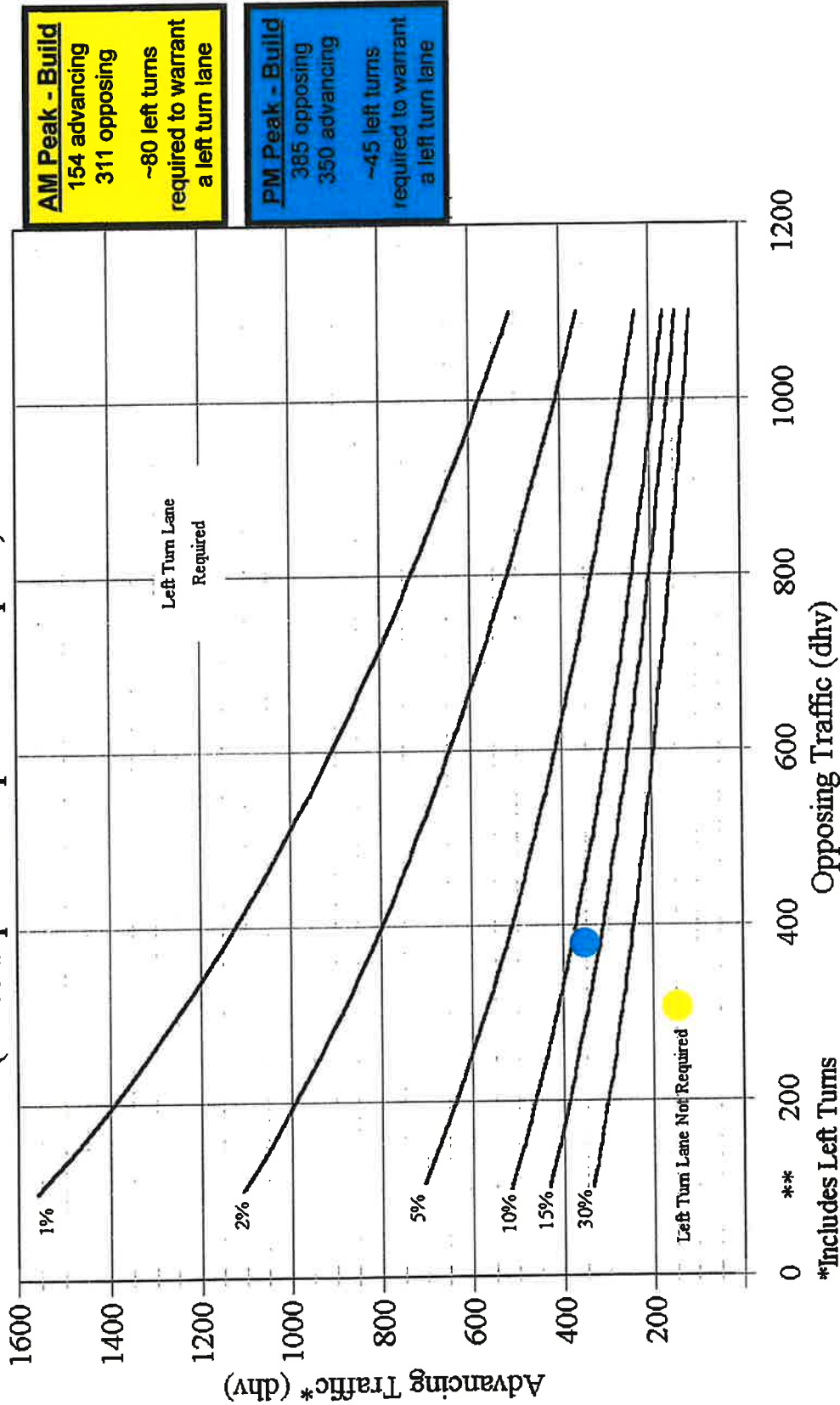
Major/Minor	Minor2	Major1	Major2
Conflicting Flow All	802	380	380
Stage 1	380	-	-
Stage 2	422	-	-
Critical Hdwy	6.43	6.23	4.13
Critical Hdwy Stg 1	5.43	-	-
Critical Hdwy Stg 2	5.43	-	-
Follow-up Hdwy	3.527	3.327	2.227
Pot Cap-1 Maneuver	352	665	1173
Stage 1	689	-	-
Stage 2	659	-	-
Platoon blocked, %			
Mov Cap-1 Maneuver	351	665	1173
Mov Cap-2 Maneuver	351	-	-
Stage 1	688	-	-
Stage 2	659	-	-

Approach	EB	NB	SB
HCM Control Delay, s	18.8	0	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	EBLn2	SBT	SBR
Capacity (veh/h)	1173	-	351	665	-	-
HCM Lane V/C Ratio	0.002	-	0.462	0.168	-	-
HCM Control Delay (s)	8.1	0	23.8	11.5	-	-
HCM Lane LOS	A	A	C	B	-	-
HCM 95th %tile Q(veh)	0	-	2.3	0.6	-	-

# Taylor Road @ TS Tech Driveway Northbound Left Turn Lane

## 2-Lane Highway Left Turn Lane Warrant (=<40 mph or 70 kph Posted Speed)



\*Includes Left Turns

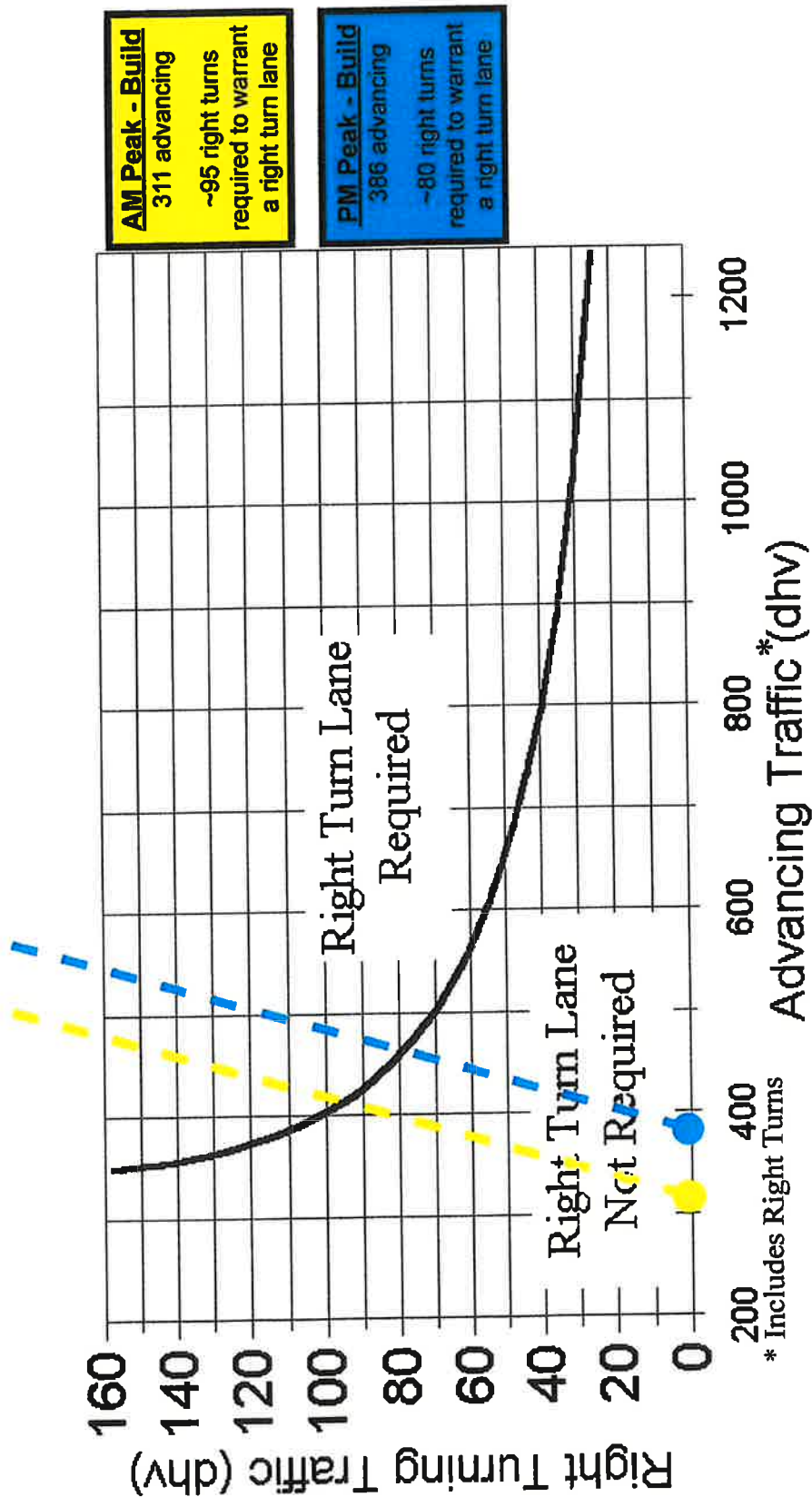
\*\* There is no minimum number of turns

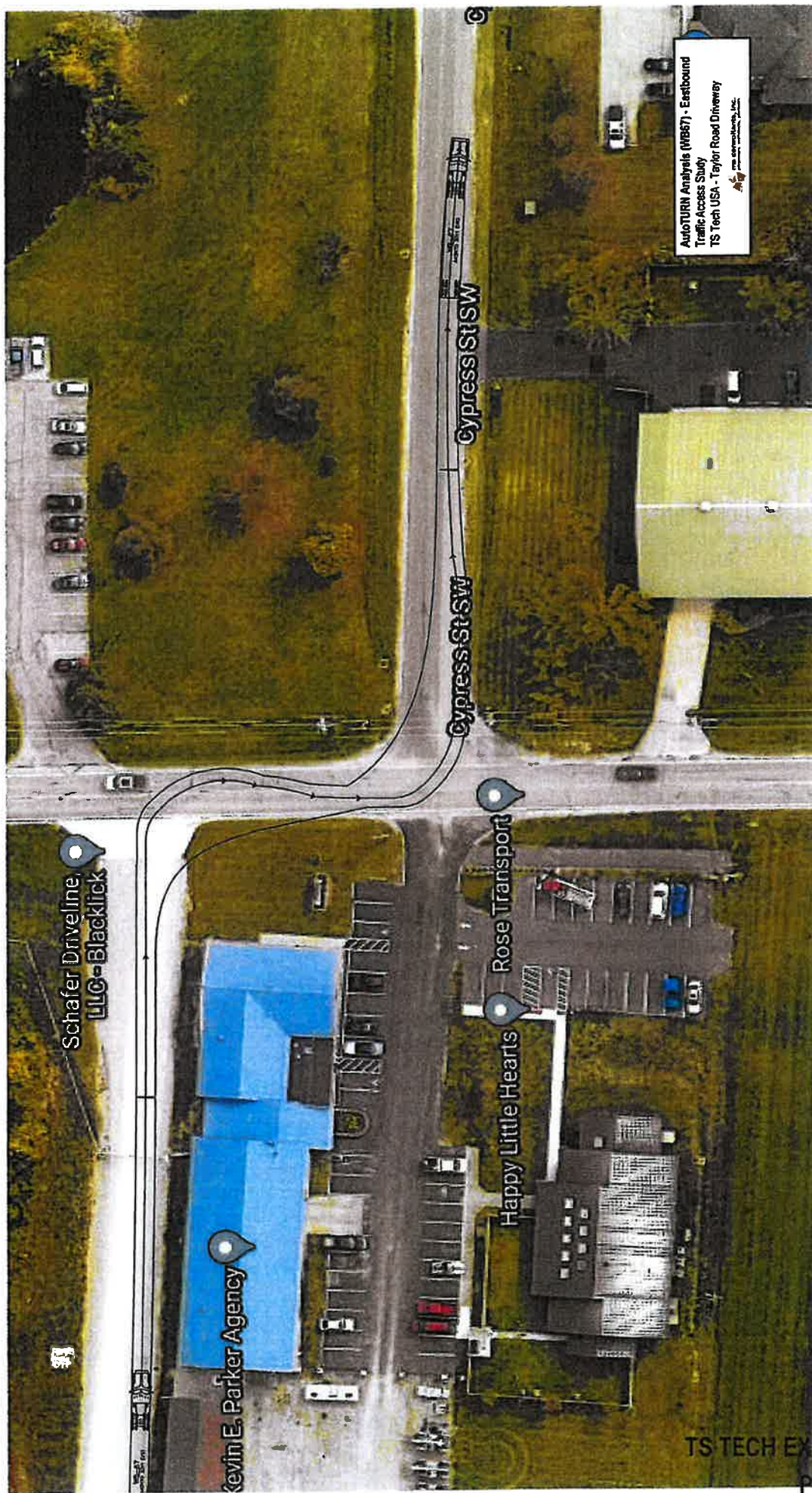
**ms consultants, inc.**  
engineers, architects, planners

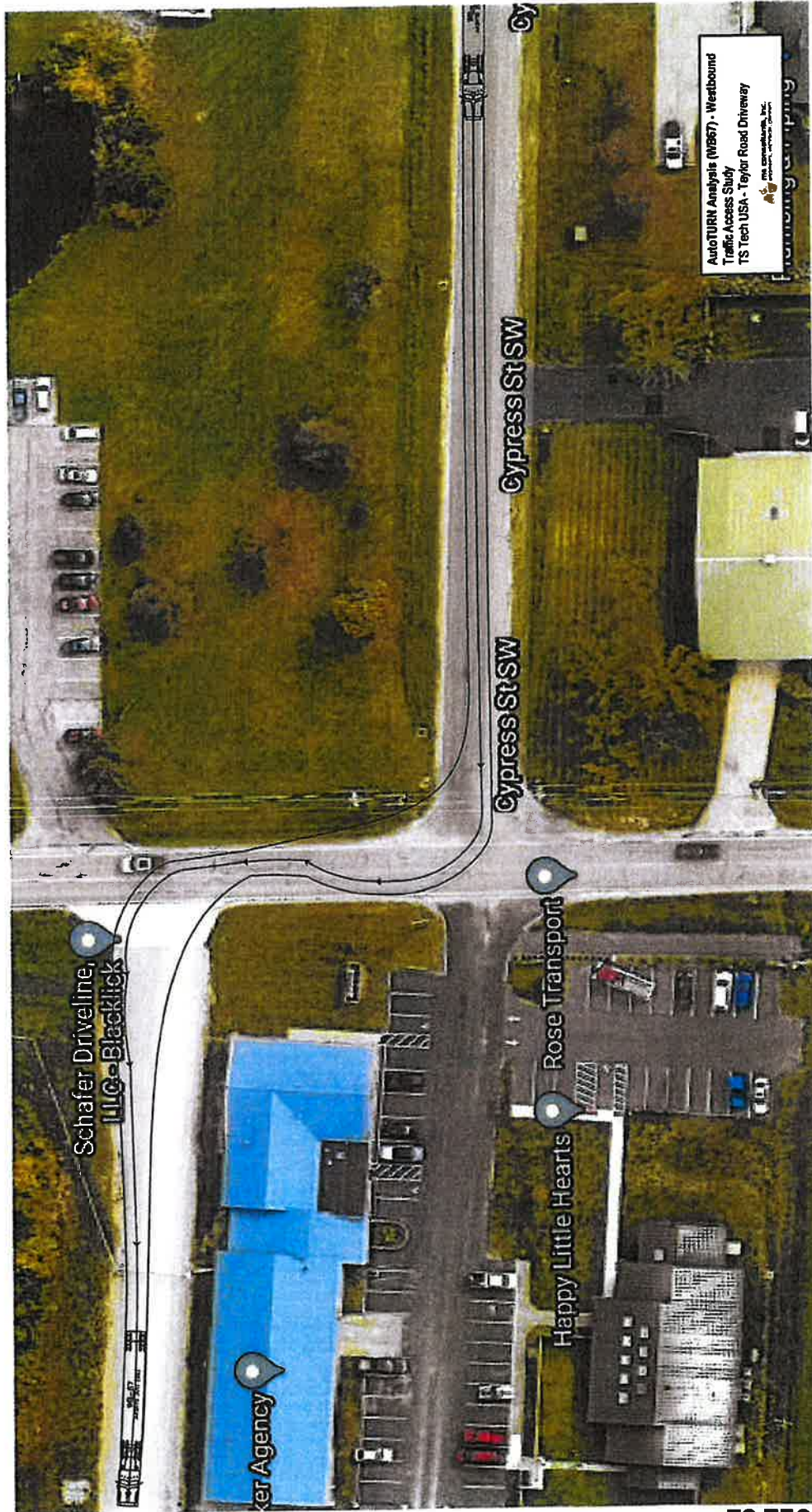


# Taylor Road @ TS Tech Driveway Southbound Right Turn Lane

## 2-Lane Highway Right Turn Lane Warrant =< 40 mph or 70 kph Posted Speed







**From:** Scott Fulton <sfulton@ci.pataskala.oh.us>  
**Sent:** Friday, January 15, 2021 10:51 AM  
**To:** Erin Wiggins  
**Cc:** Tim Hickin; Scott Hepner  
**Subject:** RE: TS Tech Drive

Erin,

Thank you for your patience. Feel free to submit a driveway permit application and any additional information on what improvements, if any, are needed.

Thanks,

**SCOTT FULTON**  
Director of Planning  
City of Pataskala  
621 West Broad Street, Suite 2-A  
Pataskala, Ohio 43062  
Phone: 740-927-2168  
Cell: 614-440-5222

**From:** erin.wiggins@tstech.com <erin.wiggins@tstech.com>  
**Sent:** Tuesday, January 12, 2021 12:26 PM  
**To:** Scott Fulton <sfulton@ci.pataskala.oh.us>  
**Cc:** Tim Hickin <thickin@ci.pataskala.oh.us>; scott.hepner@tstech.com  
**Subject:** RE: TS Tech Drive

Thank you, Scott. We look forward to hearing from you hopefully this week.

Erin Wiggins  
General Counsel/Chief Compliance Manager  
TS Tech Americas, Inc.  
8458 E. Broad St.  
Reynoldsburg, OH 43068  
Direct – 614-322-4052  
Cell – 614-282-9214  
[erin.wiggins@tstech.com](mailto:erin.wiggins@tstech.com)

**From:** Scott Fulton <sfulton@ci.pataskala.oh.us>  
**Sent:** Tuesday, January 12, 2021 11:21 AM  
**To:** Erin Wiggins <erin.wiggins@tstech.com>  
**Cc:** Tim Hickin <thickin@ci.pataskala.oh.us>; Scott Hepner <scott.hepner@tstech.com>  
**Subject:** RE: TS Tech Drive

Erin,

We are meeting this Thursday to discuss and hope to have you a response following the meeting.

Thanks,

**SCOTT FULTON**  
Director of Planning  
City of Pataskala  
621 West Broad Street, Suite 2-A  
Pataskala, Ohio 43062  
Phone: 740-927-2168  
Cell: 614-440-5222

**From:** [erin.wiggins@tstech.com](mailto:erin.wiggins@tstech.com) <[erin.wiggins@tstech.com](mailto:erin.wiggins@tstech.com)>  
**Sent:** Tuesday, January 12, 2021 10:20 AM  
**To:** Scott Fulton <[sfulton@ci.pataskala.oh.us](mailto:sfulton@ci.pataskala.oh.us)>  
**Cc:** Tim Hickin <[thickin@ci.pataskala.oh.us](mailto:thickin@ci.pataskala.oh.us)>; [scott.hepner@tstech.com](mailto:scott.hepner@tstech.com)  
**Subject:** RE: TS Tech Drive

Scott,

Following up on the below email. We would very much like to have Pataskala's thoughts on our questions below so that we can move forward together.

Thank you.

Erin Wiggins  
General Counsel/Chief Compliance Manager  
TS Tech Americas, Inc.  
8458 E. Broad St.  
Reynoldsburg, OH 43068  
Direct – 614-322-4052  
Cell – 614-282-9214  
[erin.wiggins@tstech.com](mailto:erin.wiggins@tstech.com)

**From:** Erin Wiggins  
**Sent:** Monday, January 4, 2021 10:40 AM  
**To:** Scott Fulton <[sfulton@ci.pataskala.oh.us](mailto:sfulton@ci.pataskala.oh.us)>  
**Cc:** Tim Hickin <[thickin@ci.pataskala.oh.us](mailto:thickin@ci.pataskala.oh.us)>; Scott Hepner <[scott.hepner@tstech.com](mailto:scott.hepner@tstech.com)>  
**Subject:** RE: TS Tech Drive

Scott,

I hope you had a relaxing holiday season. Thank you very much for your response below. I think we are making some progress. TS Tech has a few follow-up questions/responses to your responses so that we can make sure we are following the path Pataskala is proposing. I would appreciate your responses as soon as possible so we can continue to move forward.

- You wrote that the Permit Application should clearly identify how the drive will be utilized if the emergency-only designation is removed.
  - o The Permit Application will seek the removal of all conditions imposed by Application Permit No. 16-356. TS Tech will not propose any restrictions other than those imposed by traffic laws and ordinances. The access TS Tech is seeking will allow all traffic to enter and exit the driveway at all times in accordance with other traffic laws and ordinances.

- You wrote that Pataskala had several concerns if the emergency-only designation was removed, per Alan's Nov. 26, 2019 email.
  - o The first concern was that trucks will not be able to enter Taylor Rd. from the driveway without crossing into the northbound lane. TS Tech is not aware of this being an illegal maneuver. The Ohio Commercial Driver License Manual (found here: <https://publicsafety.ohio.gov/static/HSY7605.pdf>) illustrates trucks doing this maneuver and instructs how to do so safely. Could you please advise us of the relevant safety laws and ordinances that Pataskala is relying on to state that the potential need for such a maneuver would justify denial of the Permit Application?
  - o The second concern was that commuter traffic would stack up during shift change potentially leading to drivers risking turns. It was based partially on this concern that TS Tech commissioned the second traffic study. That study shows that commuter traffic leaving TS Tech would not degrade traffic operations on Taylor Rd. Could you please clarify if it is Pataskala's position that TS Tech would have to make some form of modifications even though the traffic study shows the driveway would not degrade traffic conditions on Taylor Rd.?

Again, thank you for helping us move this towards completion. We look forward to your response.

Erin Wiggins  
 General Counsel/Chief Compliance Manager  
 TS Tech Americas, Inc.  
 8458 E. Broad St.  
 Reynoldsburg, OH 43068  
 Direct – 614-322-4052  
 Cell – 614-282-9214  
[erin.wiggins@tstech.com](mailto:erin.wiggins@tstech.com)

**From:** Scott Fulton <[sfulton@ci.pataskala.oh.us](mailto:sfulton@ci.pataskala.oh.us)>  
**Sent:** Wednesday, December 23, 2020 12:30 PM  
**To:** Erin Wiggins <[erin.wiggins@tstech.com](mailto:erin.wiggins@tstech.com)>  
**Cc:** Tim Hickin <[thickin@ci.pataskala.oh.us](mailto:thickin@ci.pataskala.oh.us)>; Scott Hepner <[scott.hepner@tstech.com](mailto:scott.hepner@tstech.com)>  
**Subject:** TS Tech Drive

Erin,

Please see the responses below:

- At the BZA Hearing, Pataskala's position was that the "Driveway Permit Application" was the incorrect application to file for the requested upgraded use of the private driveway. However, in our December 2 meeting I understood you to say that the "Driveway Permit Application" was the necessary application. Can you please confirm? . If it's not the right application, can you please state which application, permit, or other form should be used to request upgraded use of the private driveway? Based upon the BZA's decision and direction, you should submit a Driveway Permit Application.
- At our meeting on December 2, you stated that in order for Pataskala to consider a "Driveway Permit Application" there must be some proposed, physical change which would then be the basis for the new permit application. However, I'm not able to locate the ordinance for that requirement. Could you please state the Codified Ordinance section which is the basis for this position? Having thought more about this, it is not necessary to propose a physical change in the existing driveway or emergency driveway – although you could do so if that is your plan. Given the unique situation presented by Condition Number 5 (listed in the approval for Application 16-356), you can use the Driveway Permit Application to request the emergency access designation be altered or upgraded to full access. Section 1209.02 addresses the need for a zoning permit. The Application should clearly identify how the drive will be utilized if the emergency access only designation is removed. And

please remember, when this was discussed last year, the City had several concerns if this designation was removed. Alan Haines's November 26, 2019 email (attached) outlines the City's concerns. Also note, it is your responsibility, not the City's, to engineer and design a solution or solutions to remedy the anticipated traffic issues. These solutions should be addressed in your Driveway Permit Application.

- The BZA's decision states that "TS Tech's driveway access designation can be altered if it meets all other zoning (and traffic) requirements." It does not mention the requirement for any physical upgrade in order for that to happen. Is it Pataskala's position that the current driveway does not meet relevant zoning and traffic requirements? If so, can you please identify the Codified Sections supporting those requirements as well as what changes the City will require for the driveway to meet those relevant zoning and traffic requirements? See answer above.
- At our December 2 meeting you mentioned that even a minimal physical change to the driveway could require a new driveway permit, which in Pataskala's position would start the review process. Are you able to provide us with the minimum amount of change that would need to be proposed to necessitate a new permit application? See answer above.

Happy Holidays,

**SCOTT FULTON**

Director of Planning

City of Pataskala

621 West Broad Street, Suite 2-A

Pataskala, Ohio 43062

Phone: 740-927-2168

Cell: 614-440-5222

**From:** Erin Wiggins  
**Sent:** Friday, December 11, 2020 2:32 PM  
**To:** Scott Fulton  
**Cc:** Scott Hepner  
**Subject:** RE: TS Tech - Pataskala Meeting

Scott,

Thanks to you and your team for meeting with Scott Hepner and I last week. We have now had a chance to discuss that meeting as well as review the BZA written opinion. We would like to move this process forward and, as you had offered, we have some follow-up questions for you. We would appreciate your prompt response to allow us time to draft the needed paperwork.

- At the BZA Hearing, Pataskala's position was that the "Driveway Permit Application" was the incorrect application to file for the requested upgraded use of the private driveway. However, in our December 2 meeting I understood you to say that the "Driveway Permit Application" was the necessary application. Can you please confirm? If it's not the right application, can you please state which application, permit, or other form should be used to request upgraded use of the private driveway?
- At our meeting on December 2, you stated that in order for Pataskala to consider a "Driveway Permit Application" there must be some proposed, physical change which would then be the basis for the new permit application. However, I'm not able to locate the ordinance for that requirement. Could you please state the Codified Ordinance section which is the basis for this position?
- The BZA's decision states that "TS Tech's driveway access designation can be altered if it meets all other zoning (and traffic) requirements." It does not mention the requirement for any physical upgrade in order for that to happen. Is it Pataskala's position that the current driveway does not meet relevant zoning and traffic requirements? If so, can you please identify the Codified Sections supporting those requirements as well as what changes the City will require for the driveway to meet those relevant zoning and traffic requirements?
- At our December 2 meeting you mentioned that even a minimal physical change to the driveway could require a new driveway permit, which in Pataskala's position would start the review process. Are you able to provide us with the minimum amount of change that would need to be proposed to necessitate a new permit application?

As you know, TS Tech is eager to bring this process to conclusion. Based on our December 2 conversation we understand Pataskala's position to be that a new "Driveway Permit Application" must be filed proposing some minor, physical change. We currently intend to file that permit application during the first week of January 2021 unless we hear differently from you. We ask that you provide us answers to the above questions by December 23, 2020 to allow us time to craft the proper application.

Thank you.

Erin Wiggins  
General Counsel/Chief Compliance Manager  
TS Tech Americas, Inc.  
8458 E. Broad St.  
Reynoldsburg, OH 43068  
Direct – 614-322-4052  
Cell – 614-282-9214  
erin.wiggins@tstech.com

**From:** Scott Fulton <[sfulton@ci.pataskala.oh.us](mailto:sfulton@ci.pataskala.oh.us)>  
**Sent:** Tuesday, December 1, 2020 3:15 PM  
**To:** Erin Wiggins <[erin.wiggins@tstech.com](mailto:erin.wiggins@tstech.com)>  
**Cc:** Scott Hepner <[scott.hepner@tstech.com](mailto:scott.hepner@tstech.com)>  
**Subject:** RE: TS Tech - Pataskala Meeting

Erin,

I was referring to the conditions placed in 2016 for the emergency access and in 2019 for the cross dock.

Thanks,

**SCOTT FULTON**  
Director of Planning  
City of Pataskala  
621 West Broad Street, Suite 2-A  
Pataskala, Ohio 43062  
Phone: 740-927-2168  
Cell: 614-440-5222

**From:** [erin.wiggins@tstech.com](mailto:erin.wiggins@tstech.com) <[erin.wiggins@tstech.com](mailto:erin.wiggins@tstech.com)>  
**Sent:** Tuesday, December 1, 2020 3:08 PM  
**To:** Scott Fulton <[sfulton@ci.pataskala.oh.us](mailto:sfulton@ci.pataskala.oh.us)>  
**Cc:** [scott.hepner@tstech.com](mailto:scott.hepner@tstech.com)  
**Subject:** RE: TS Tech - Pataskala Meeting

Scott,

I want to make sure I understand the argument you made below. Are you referring to TS Tech not appealing the driveway restriction within 30 days of the restriction being originally placed (2016)? Or are you referring to an appeal not being filed within 30 days of the most recent building permits? Or something else entirely?

Thank you.

Erin Wiggins  
General Counsel/Chief Compliance Manager  
TS Tech Americas, Inc.  
8458 E. Broad St.  
Reynoldsburg, OH 43068  
Direct – 614-322-4052  
Cell – 614-282-9214  
[erin.wiggins@tstech.com](mailto:erin.wiggins@tstech.com)

**From:** Scott Fulton <[sfulton@ci.pataskala.oh.us](mailto:sfulton@ci.pataskala.oh.us)>  
**Sent:** Tuesday, December 1, 2020 2:49 PM  
**To:** Erin Wiggins <[erin.wiggins@tstech.com](mailto:erin.wiggins@tstech.com)>  
**Cc:** Scott Hepner <[scott.hepner@tstech.com](mailto:scott.hepner@tstech.com)>  
**Subject:** RE: TS Tech - Pataskala Meeting

Erin,

Thank you for your patience while we have discussed how to move this forward. Due to the appeals not having been filed within 30 days of the permits being issued, we have had to think outside the box and have had numerous discussions on how to proceed. We do think there is an option; however, we will need TS Tech to identify the exact intended use of the driveway. Our plan for the meeting tomorrow is to present you with this option, discuss the use and continue the process of identifying the traffic concerns and possible solutions.

Thanks,

**SCOTT FULTON**  
Director of Planning  
City of Pataskala  
621 West Broad Street, Suite 2-A  
Pataskala, Ohio 43062  
Phone: 740-927-2168  
Cell: 614-440-5222

**From:** [erin.wiggins@tstech.com](mailto:erin.wiggins@tstech.com) <[erin.wiggins@tstech.com](mailto:erin.wiggins@tstech.com)>  
**Sent:** Monday, November 30, 2020 10:38 AM  
**To:** Scott Fulton <[sfulton@ci.pataskala.oh.us](mailto:sfulton@ci.pataskala.oh.us)>  
**Cc:** [scott.hepner@tstech.com](mailto:scott.hepner@tstech.com)  
**Subject:** RE: TS Tech - Pataskala Meeting

Scott,

We look forward to meeting you on Wednesday afternoon at our facility. In advance of that meeting, please provide us with Pataskala's position on the next steps that need to occur to get the driveway restrictions lifted. You mentioned these steps in response to a question at the BZA hearing which is why the BZA instructed us to work together to find a solution. We would appreciate hearing those steps in advance so we can have a more productive discussion on Wednesday.

Thank you.

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[erin.wiggins@tstech.com](mailto:erin.wiggins@tstech.com)

**From:** Erin Wiggins  
**Sent:** Wednesday, November 18, 2020 4:14 PM  
**To:** Scott Fulton <[sfulton@ci.pataskala.oh.us](mailto:sfulton@ci.pataskala.oh.us)>  
**Cc:** Scott Hepner <[scott.hepner@tstech.com](mailto:scott.hepner@tstech.com)>  
**Subject:** RE: TS Tech - Pataskala Meeting

Scott,

Let's meet at 2 pm on 12/2. I will send a meeting invite to you and Alan which you can forward as needed. You can come to TS Tech Americas, which is the building to the right as you come north on TS Tech drive. Please tell the guard you are going to TS Tech Americas to meet with me.

Just a reminder, please do not come if you are feeling sick or if you've been around someone who is sick. You will all be asked to fill out an attestation to that effect. Please also remember to bring a mask, but we have spare masks if you forget.

Let's plan to meet in person but if conditions get worse and our counties issue stay at home orders let's keep the meeting time and we will meet virtually.

Regarding your question about the currently-intended use of the Taylor Rd. driveway, Scott Hepner's image is an accurate depiction of that currently-intended use. However, TS Tech's position continues to be that it should have no restrictions so long as the property is used as zoned by Pataskala. Therefore, our statement of currently-intended use is not an agreement that limitations can be placed on the drive. I hope that allows Pataskala enough information to discuss internally before our meeting. Please let me know if you need more information.

At the BZA hearing, you indicated to the BZA that there were a number of steps that would need to happen in order for Pataskala to agree to lift the current restrictions. Could you please provide any information on that to us in advance of the meeting so that we can also internally discuss?

Thank you.

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From: Scott Fulton <[sfulton@ci.pataskala.oh.us](mailto:sfulton@ci.pataskala.oh.us)>  
Sent: Wednesday, November 18, 2020 2:38 PM  
To: Erin Wiggins <[erin.wiggins@tstech.com](mailto:erin.wiggins@tstech.com)>  
Cc: Scott Hepner <[scott.hepner@tstech.com](mailto:scott.hepner@tstech.com)>  
Subject: RE: TS Tech - Pataskala Meeting

Erin,

I spoke with Tim Hickin and Alan Haines and we are available at 2pm on either 12/2 or 12/3. We're happy to meet at TS Tech.

Following our most recent meeting with Scott, he sent over the attached proposal for usage of the driveway. I just want to confirm that the proposal is still the same so we can provide some comments for the meeting.

Thanks,

SCOTT FULTON  
Director of Planning  
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621 West Broad Street, Suite 2-A  
Pataskala, Ohio 43062  
Phone: 740-927-2168  
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**From:** [erin.wiggins@tstech.com](mailto:erin.wiggins@tstech.com) <[erin.wiggins@tstech.com](mailto:erin.wiggins@tstech.com)>  
**Sent:** Wednesday, November 18, 2020 8:33 AM  
**To:** Scott Fulton <[sfulton@ci.pataskala.oh.us](mailto:sfulton@ci.pataskala.oh.us)>  
**Cc:** [scott.hepner@tstech.com](mailto:scott.hepner@tstech.com)  
**Subject:** RE: TS Tech - Pataskala Meeting

Scott,

We can be available at any of the below times. I'd appreciate if we could select a date/time as soon as possible so that Scott Hepner and I can make sure we block off our schedules. Like before, we propose that TS Tech host this meeting so that we have access to the cross-dock as needed.

11/30 – morning  
12/1 – morning  
12/2 – all day  
12/3 – afternoon

The BZA mentioned that the point of this meeting was to discuss the process you/Pataskala proposes TS Tech needs to use to get the driveway restrictions lifted. If you could send along your thoughts on that in advance of our meeting it will help us make the meeting as productive as possible.

Thank you.

Erin Wiggins  
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[erin.wiggins@tstech.com](mailto:erin.wiggins@tstech.com)

**From:** Scott Fulton <[sfulton@ci.pataskala.oh.us](mailto:sfulton@ci.pataskala.oh.us)>  
**Sent:** Tuesday, November 17, 2020 4:18 PM  
**To:** Erin Wiggins <[erin.wiggins@tstech.com](mailto:erin.wiggins@tstech.com)>  
**Cc:** Scott Hepner <[scott.hepner@tstech.com](mailto:scott.hepner@tstech.com)>  
**Subject:** RE: TS Tech - Pataskala Meeting

Erin,

I've been called into court on Thursday, so unfortunately that day is out. What dates/times work for you gentlemen next week?

Thanks,

**SCOTT FULTON**  
Director of Planning  
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**From:** [erin.wiggins@tstech.com](mailto:erin.wiggins@tstech.com) <[erin.wiggins@tstech.com](mailto:erin.wiggins@tstech.com)>  
**Sent:** Tuesday, November 17, 2020 3:26 PM  
**To:** Scott Fulton <[sfulton@ci.pataskala.oh.us](mailto:sfulton@ci.pataskala.oh.us)>  
**Cc:** [scott.hepner@tstech.com](mailto:scott.hepner@tstech.com)  
**Subject:** RE: TS Tech - Pataskala Meeting

Scott,

Will a meeting this Thursday (11/19) afternoon be possible? Have you heard back from the other Pataskala officials who want to join?

Please let us know at your earliest convenience.

Thank you.

Erin Wiggins  
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Cell – 614-282-9214  
[erin.wiggins@tstech.com](mailto:erin.wiggins@tstech.com)

**From:** Scott Fulton <[sfulton@ci.pataskala.oh.us](mailto:sfulton@ci.pataskala.oh.us)>  
**Sent:** Friday, November 13, 2020 11:17 AM  
**To:** Erin Wiggins <[erin.wiggins@tstech.com](mailto:erin.wiggins@tstech.com)>  
**Cc:** Scott Hepner <[scott.hepner@tstech.com](mailto:scott.hepner@tstech.com)>  
**Subject:** RE: TS Tech - Pataskala Meeting

Erin,

Waiting to hear back from other folks in our office on dates/times. I will let you know once I hear back from them.

Thanks,

SCOTT FULTON  
Director of Planning  
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Cell: 614-440-5222

**From:** [erin.wiggins@tstech.com](mailto:erin.wiggins@tstech.com) <[erin.wiggins@tstech.com](mailto:erin.wiggins@tstech.com)>  
**Sent:** Friday, November 13, 2020 10:57 AM  
**To:** Scott Fulton <[sfulton@ci.pataskala.oh.us](mailto:sfulton@ci.pataskala.oh.us)>  
**Cc:** [scott.hepner@tstech.com](mailto:scott.hepner@tstech.com)  
**Subject:** RE: TS Tech - Pataskala Meeting

Scott,

Following up on my email below. Please let me know which day next week works best for you and who you plan to bring with you. We look forward to moving this process forward.

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[erin.wiggins@tstech.com](mailto:erin.wiggins@tstech.com)

From: Erin Wiggins  
Sent: Wednesday, November 11, 2020 11:10 AM  
To: [sfulton@ci.pataskala.oh.us](mailto:sfulton@ci.pataskala.oh.us)  
Cc: Scott Hepner <[scott.hepner@tstech.com](mailto:scott.hepner@tstech.com)>  
Subject: TS Tech - Pataskala Meeting

Scott,

As discussed last night, TS Tech would like to meet with you in person next week to discuss the restrictions on TS Tech's Taylor Rd. driveway. At that meeting, we would like to finalize the process that Pataskala asserts would be the proper path for TS Tech to take. We can be available the following days:

Monday (11/16): Anytime  
Thursday (11/19): After 1 pm

Please let me know what times work best for you.

We propose that TS Tech hosts the meeting so that we can examine the cross-dock/driveway as needed. Please feel free to invite anyone else that you feel is necessary but please let me know who is coming so that I can send along our COVID protocol information.

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